



Planning and Highways Committee

Date: Thursday, 2 September 2021

Time: 2.00 pm

Venue: Council Chamber, Level 2, Town Hall Extension

This is a supplementary agenda containing additional information about the business of the meeting that was not available when the agenda was published

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Membership of the Planning and Highways Committee

Councillors

Curley (Chair), Shaukat Ali, Andrews, Baker-Smith, Y Dar, Davies, Hutchinson, Kamal, Kirkpatrick, J Lovecy, Lyons, Riasat, Richards and Stogia

Supplementary Agenda

10. **130394/FO/2021 & 130395/LO/2021 - Brunswick Place,
Bradford Road, Manchester, M40 7EZ - Ancoats & Beswick
Ward** 3 - 86

The report of the Director of Planning, Building Control and
Licensing is enclosed.

Further Information

For help, advice and information about this meeting please contact the Committee Officer:

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This supplementary agenda was issued on **Wednesday, 25 August 2021** by the
Governance and Scrutiny Support Unit, Manchester City Council, Level 3, Town Hall
Extension (Lloyd Street Elevation), Manchester M60 2LA

Application Number	Date of Appln	Committee Date	Ward
130394/FO/2021 and 130395/LO/2021	26 May 2021	2 Sept 2021	Ancoats & Beswick Ward

Proposal Refurbishment, removal/demolition, repair and reconfiguration of Brunswick Mill to create work spaces, retail and community uses (Use Class E and Use Class F2) (2034 sqm) at the ground floor and creation of 153 residential apartments (Use Class C3a) with the upper floors following demolition works together with the erection of a part 6, part 8 storey building to form 100 residential apartments (Use Class C3a) and a 5 storey buildings to form 24 residential apartments (Use Class C3a) (277 apartments in total across the 3 buildings) with associated car parking, roof top amenity space, access and servicing, landscaping, pedestrian access to the Ashton Canal and other associated works

Listed Building Consent for refurbishment, removal/demolition, repair and reconfiguration of Brunswick Mill including deconstruction of the small vestiges of perimeter walls of the former cotton mill immediately west of Brunswick Mill (known as Pooley's Mill) to create work spaces, retail and community uses (Use Class E and Use Class F2) (2034 sqm) at the ground floor and creation of 153 residential apartments (Use Class C3a) with the upper floors following demolition works together with the erection of a part 6, part 8 storey building to form 100 residential apartments (Use Class C3a) and a 5 storey buildings to form 24 residential apartments (Use Class C3a) (277 apartments in total across the 3 buildings) with associated car parking, roof top amenity space, access and servicing, landscaping, pedestrian access to the Ashton Canal and other associated works

Location Brunswick Place, Bradford Road, Manchester, M40 7EZ

Applicant Brunswick Property Investments Ltd, C/o Agent

Agent Mrs Rachel Brown, Deloitte LLP, The Hanover Building, Manchester

EXECUTIVE SUMMARY

The proposal is for planning and listed building consent to create 153 homes in the refurbished Brunswick Mill and the erection of a part 6, part 8 storey building to form 100 homes and a 5 storey building to form 24 homes (Use Class C3a) creating 277 homes in total.

5 objections have been received.

Key Issues

Principle of the proposal and the schemes contribution to regeneration The development accords with national and local planning policies, and would bring

significant economic, social and environmental benefits. This brownfield site contains a grade II listed building which has been in decline for a number of years. Large parts are derelict, open to the elements and require significant investment.

The 277 homes for open market sale would be close to public transport, walking and cycle routes. 20% of the 81 parking spaces would be fitted for an electric car with all spaces adaptable. There would be 100% cycle provision and enhanced public realm including a pedestrian link through the site to the Ashton Canal towpath.

Economic 152 direct jobs would be created during construction with a further 224 off site jobs created through the supply chain. These equate to £24.2 million per year for the duration of the development. 156 jobs would be created in the workspaces and commercial floorspace with an annual GVA of £5.8 million per year. The listed building would be restored and repaired.

Social A local labour agreement would ensure that Manchester residents are prioritised for construction jobs. The scheme would stabilise and repair the listed building and introduce natural surveillance along and enhance pedestrian links to the Ashton Canal Towpath.

Environmental This would be a low carbon development in a highly sustainable location. It would be highly efficient and meet some of its energy needs through renewable technology. There are no harmful impacts on traffic and local air quality and any impacts can be mitigated. The ground conditions are not complex or unusual and drainage aims to minimise surface water run off. The height, scale and appearance of the new building would respect the setting of the listed building. Secured by Design principles would ensure the development is safe and secure. Waste management would prioritise recycling. .

Impact on the historic environment The proposal is viable and deliverable. The refurbishment works would result in less than substantial harm to the significance of the listed building. Highly significant features would be retained, repaired and revealed. A clear and convincing justification has been presented to justify the level of harm caused to the building along with identified public benefits of the scheme.

Impact on local residents and local businesses The impact on daylight/sunlight and overlooking are considered to be acceptable. Construction impacts would not be significant and can be managed to minimise the effects on local businesses. Noise outbreak from plant and the commercial unit would meet relevant standards.

A full report is attached below for Members consideration.

Description

This 0.87 hectares site is bounded by Bradford Road, the Ashton Canal and Brunswick Street. It comprises 3 plots, namely: the Brunswick Mill complex; a vacant site to the south west of the Mill on Bradford Road; and land at the junction of Bradford Road and Brunswick Street. Brunswick Mill is a grade II listed building which is used by a number of light industrial uses and warehousing. There are 11 tenants ranging from music studios, supply and repair workshop and storage.

The mill chimney was removed in the 1970's and India Mills (or Pooleys Mill) to the west was demolished in 2008. Brunswick Mill is in a poor state of repair. Few original windows remain and those that do are in a poor state of repair resulting in water ingress and damp. Extensive masonry repairs are needed. The other plots are vacant.

The site is in Holt Town Central area as defined in the Eastlands Update (2019) and is located between the Etihad Campus, Miles Platting and the Ancoats and New Islington neighbourhoods and is at a strategically important link between the campus and the city centre.

There are established residential areas in Miles Platting and New Islington on the opposite side of Bradford Road and Beswick Street. There are commercial buildings and services yards including the Pretty Little Thing HQ in Wellington Mill on the opposite side of the canal. The Ashton Canal is a pedestrian and cycling route. This is a sustainable and accessible location, well connected to the City Centre, the Etihad Campus and adjoining neighbourhoods. Metrolink stops are nearby.

The site is not in a conservation area but the following listed buildings are nearby: The Ashton Canal Bridge Number 5 (Grade II), Spectator Mill (Grade II), Hope Mill (Grade II*) and the Ashton Canal Bridge number 4 (Grade II).

The site is in Flood Zone 1 and a critical drainage area. It is in the Manchester Air Quality Management Area (AQMA) where air quality conditions are poor.

Planning History

083665/FO/2007/N2: Refurbishment and conversion of Brunswick Mill to form 126 no. residential units. Erection of part 4 , part 8, part 9 storey building and an 11 storey building facing onto Bradford Road, comprising 130 no. apartments and 8 no. town houses, Class A1 retail use (139sqm) Class A2 use (1442sqm) and Class B1 office use (895sqm) (2337sqm in total) with associated carparking at ground and basement levels, together with ancillary landscaping, following demolition of outbuildings and annexes to existing Brunswick Mill Withdrawn 12 September 2008

119268/DEM/2018: Prior notification of demolition of buildings and structures Prior Approval 03 Apr 2018

The Proposal

The proposal would convert the mill and redevelop the adjacent land to create 277 new homes and 2034 sqm of work spaces, retail and community uses. Brunswick Mill would contain work spaces at the ground and part first floor level aimed at creative, community and employment uses. The remainder of the building would contain 153 homes with 57 one bed, 89 two bed and three 3 bed apartments.

The mill would be repaired and refurbished with some demolition which is considered in the report. New windows would be provided and a landscaped courtyard created.

A part 8, part 6 storey 'Mid' building would be erected on Bradford Road containing 100 homes with 13 townhouses (10 two bed, 2 three bed and 1 four bed) at ground and first floor level and 87 homes above (32 one bed and 55 two bed).

A 5 storey building at the corner of Bradford Road and Beswick Street would contain a commercial unit on the ground floor and 24 homes above, with 8 one bed and 16 two bed apartments.

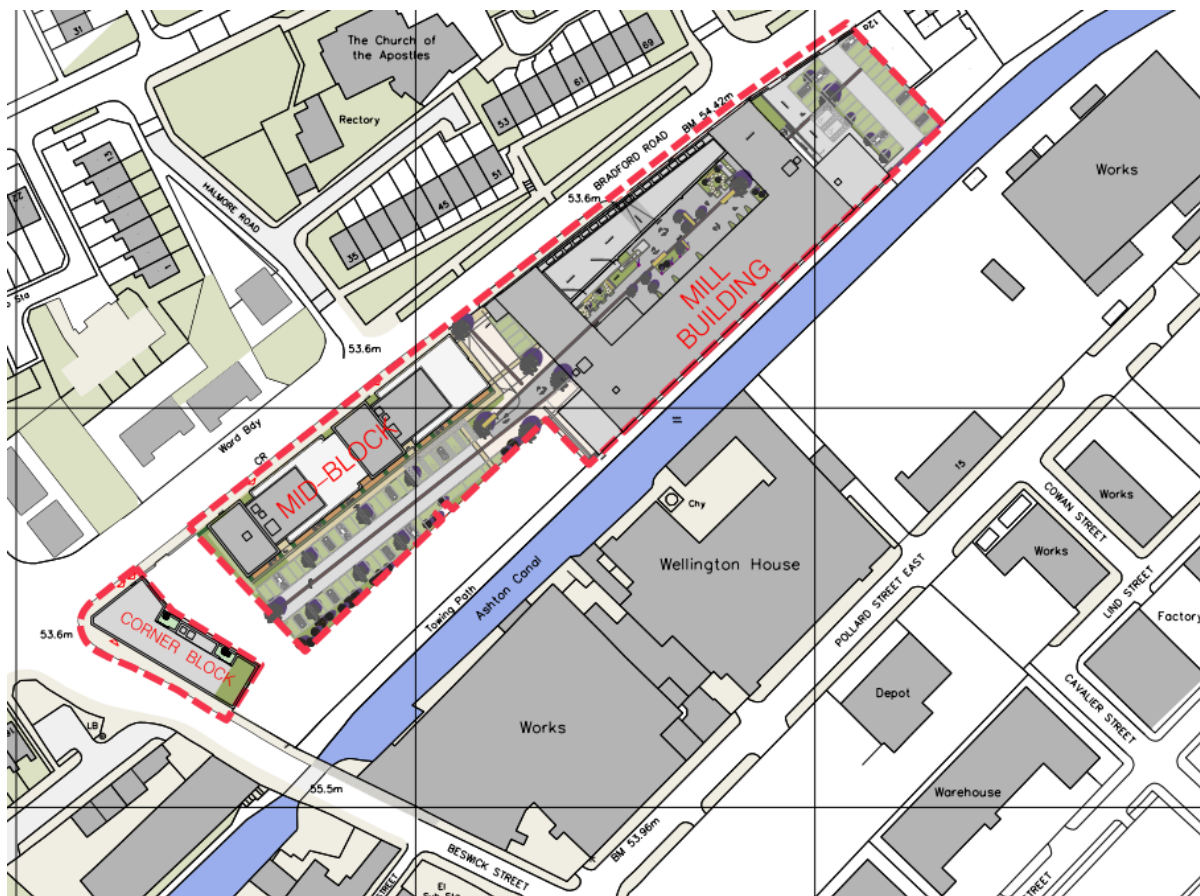
The new buildings seeks to replicate the regular form and masonry elements of the listed building. A regular pattern of windows would be created in deep reveals with horizontal bands and hit and miss panels, which also help to conceal air vents. Zinc cladding at the upper levels would be set back from the masonry.



Image of the mill conversion and new build elements

Three vehicular access points are proposed off Bradford Road. Two access points to the north are to the Mill car park. That to the south is to the new build car park. The cycle stores would be accessed from Bradford Road and the car parks. Pedestrians to the Mill would enter off the landscaped courtyard via the arch off Bradford Road. The ground floor commercial uses would have independent access off the courtyard.

Comprehensive landscaping includes reclaimed cobbles, trees and soft landscaping with access to the Ashton Canal towpath.



Layout of the site

The Planning Submission

This planning and Listed Building applications have been supported by the following information:

- Structural drawings;
- Landscape Plan;
- Design and Access Statement;
- Landscape and Public Realm Strategy;
- Daylight and Sunlight Report;
- Heritage Assessment;
- Window Condition Report;
- Archaeological Assessment;
- Crime Impact Statement;
- Statement of Consultation;
- Travel Plan;
- Transport Statement;
- Waste Management Strategy;
- Ecological Survey;
- Energy Statement and ESS;
- Ventilation Report;
- Utilities Report;
- Broadband Assessment;
- Flood Risk and Drainage Statement;

- Noise Statement;
- Management Strategy;
- Air Quality Statement;
- TV Reception Survey;
- Desk Top Geo-Environmental Survey;
- Construction Methodology Statement;
- Structural Statement;
- Feasibility Report;
- Local Labour Agreement; and
- Viability Report.

Consultations

The proposal has been advertised as a major development, as being of public interest, as affecting the setting of a Listed Building and listed building consent. A Site notice was displayed and a notice placed in the local press. Notification letters have been sent to an extensive area of local residents and businesses and five letters of objection have been received.

- There is no affordable housing and local residents can't afford to buy homes. This proposal is gentrification;
- Brunswick Mill is a creative hub for musicians and one of the only places musicians can have permanent rehearsal spaces. Transforming the mill into more flats will have a devastating impact on the already devastated music scene in Manchester;
- The Mill is character filled and its heritage should be retained along with the cultural spaces and not at the expense of more flats;
- The ERF suggests development up to eight storeys within the Holt Town but this site is in Miles Platting, north of the Ashton Canal, adjacent to a low rise residential neighbourhood and is unacceptable;
- The erection of an 8 storey building, approximately 1 metre lower than the mill, would not respond to place or respect the iconic asset;
- The comparison to Murray Mills is misleading as it is in the city centre;
- While the flexibility of the ERF does allow for high density housing in Holt Town the location north of the Canal and next to the mill should prevent such a proposal; The proposal should be for low rise accommodation at 2-3 storeys;
- There has been over a year of disruption due to other development work in the area. This disruption has involved early mornings on Sundays and should not happen as part of this proposal;
- The development is excessive. Although something needs to be done to the building, there are current small businesses within the building and one of the last few places for start ups and creatives;
- The noise pollution would be excessive and detrimental to health and wellbeing along with businesses which operate close by;
- The proposal would block light to the houses opposite.

Highway Services the level of car parking is acceptable, with 20% fitted with electric vehicle charging points and the remaining spaces being adapted at a future date. The pedestrian and vehicle entrance points and servicing arrangements are acceptable. Off site highways works are required to ensure that the vehicular and

pedestrian environment is safe. A travel plan and construction management plan should be agreed.

Environmental Health recommends conditions regarding construction management, lighting and control of glare, glazing specifications, plant and acoustic insulation. The waste management strategy and air quality assessment are acceptable. Further ground investigations are required including a verification report on completion.

Works and Skills Team recommend a local labour scheme condition.

Flood Risk Management details of a surface water drainage scheme should be approved together with a management regime and verification report.

Neighbourhood Services (Trees) have no objections as no trees would be lost.

Environment Agency the site area has been used for industrial activities which could have introduced adverse contamination. Controlled waters are particularly sensitive as the site overlies a Secondary A Aquifer associated with the underlying bedrock formation. The superficial cover comprises Glacial Till which is a secondary undifferentiated aquifer. The Ashton Canal, a sensitive potential receptor, runs adjacent to the development site's southern boundary. Details of the remediation strategy and piling should be agreed to minimise the risk to controlled waters.

Greater Manchester Ecology Unit advise that the bat method statement is acceptable provided the Council is provided with confirmation of a licence from Natural England, that measures are put in place to protect the Ashton Canal from construction activities and biodiversity measures are agreed.

Historic England have no objection to the conversion of Brunswick Mill which would provide heritage benefits if it ensured that the building had a sensitive viable use.

There no objection to the overall principle of the works, but consideration should be given to the exact details, including the detailed drawings showing the new partitions, windows and works to the jack arches. The subdivision would have a negative impact on the historic significance of the Mill but if this is the minimum necessary to secure this use they would conclude that the heritage benefits would outweigh this harm.

There are no objections to most of the demolition but the transformer station has historic interest, and is an important element of the complex's evolution. The applicant should seeks to retain and convert this element or provide a clear justification for why it cannot be viably retained.

The new build would not be a sensitive addition to the environment in which the listed building is experienced, forming a jarring and visually distracting intrusion. Its harm on the setting of the listed building would be less than substantial to its significance. The massing and design should be modified to avoid its currently jarring and distracting nature.

It is also highlighted that the adjacent site was previously occupied by Pooley's Mill which should be the subject of further archaeological investigations.

Canal and River Trust the new access to the towpath is positive and would encourage regular use of the canal corridor. Further details are required regarding materials and access and safety arrangements. A method statement is required on the impact on the canal wall. A construction management plan should ensure that pollutants do not enter the canal. Bats should be protected and lighting should not affect habitats. The development should include biodiversity improvements.

Greater Manchester Archaeology Advisory Service (GMASS) advise that the site was developed initially in the 1820s when a large steam powered cotton mill known as Pooley's Mill was erected across the south western half of the application. There are likely to be remains of archaeological interest associated with this which would merit further investigation. The demolition works should be recorded as part of a building investigation survey. Planning conditions are required.

Design for Security at Greater Manchester Police a condition should require the development to be carried out in accordance with the Crime Impact Statement.

Policy

The Development Plan

The Development Plan consists of The Manchester Core Strategy (2012); and Saved policies of the Unitary Development Plan for the City of Manchester (1995). The Core Strategy is the key document in Manchester's Local Development Framework and sets out the long-term strategic planning policies for Manchester's future development.

A number of UDP policies have been saved until replaced by further development plan documents to accompany the Core Strategy. Planning applications in Manchester must be decided in accordance with the Core Strategy and saved UDP policies as directed by section 38 (6) of the Planning and Compulsory Purchase Act 2004 unless material considerations indicate otherwise.

The relevant policies within the Core Strategy are as follows:

Strategic Spatial Objectives - The adopted Core Strategy contains Strategic Spatial Objectives that form the basis of its policies, as follows:

Manchester Core Strategy Development Plan Document (July 2012)

The relevant policies within the Core Strategy are as follows:

SO1. Spatial Principles –The proposal would deliver high quality homes, workspaces, commercial and community spaces in a highly sustainable location in a strategic regeneration area.

SO2. Economy – High quality homes in this sustainable location would support economic growth and new commercial and community spaces would support job creation. The construction would create local job opportunities,

S06. Environment – The development would be low carbon and highly sustainable using up to date energy efficiency measures in the fabric and construction. There would be a travel plan and 100% cycle provision.

Policy SP1 ‘Spatial Principles – The proposal would improve visual amenity and restore a Listed Building. This would contribute positively to the street scene and complement development in the area.

Policy EC3 ‘The Regional Centre’, Primary Economic Development Focus (City Centre and Fringe and Policy CC8 Change and Renewal– The proposal would provide homes close to all forms of sustainable transport.

Policy CC9 Design and Heritage – The proposal provides high quality buildings in Holt Town through the refurbishment of a local heritage landmark and listed building and provision of new buildings within its setting.

Policy CC10 A Place for Everyone – The proposal would complement the ongoing regeneration of East Manchester. There are constraints due to this being a listed building. Level access is proposed with lifts in the listed building. Parking would be provided for disabled people.

Policy T1 ‘Sustainable Transport’ – There is access to all public transport modes including Holt Town and New Islington tram stops.

Policy T2 ‘Accessible areas of opportunity and needs’ - A transport assessment and travel plan demonstrate that the proposal would have minimal impact on the local highway network and would encourage the use of sustainable transport.

Policy H1 ‘Overall Housing Provision’ – This is a high-density development on a previously developed site in a highly sustainable location. The range of accommodation sizes is appropriate in this instance given the constraints of the listed building. Amenity spaces and cycle and waste management arrangements would ensure this is a sustainable and high quality development.

Policy H2 ‘Strategic Housing Location’ – The proposal would develop a brownfield site in East Manchester and deliver good quality accommodation in a highly sustainable area. The fabric would be efficient with sustainable features such as photovoltaics and sustainable drainage are included.

Policy H4 ‘East Manchester’ – The proposal would provide high density homes. The homes would be a mixture of one and two beds with larger townhouses and apartments. The development would cater for a range of household needs and be suitable for families.

Policy H8 ‘Affordable Housing’ – The proposal would not provide any affordable housing due to viability constraints. This has been independently tested. The viability would be re-tested at an agreed date in the future to determine if the viability has improved and a contribution can be sought.

Policy EN1 'Design principles and strategic character areas' - This high quality scheme would enhance the regeneration of the area.

Policy EN3 'Heritage' - The impact on the historic environment would be acceptable and this is considered in further detail in the report.

EN4 'Reducing CO₂ emissions by enabling low and zero carbon development' – The proposal would have energy efficient fabric. A travel plan and cycle provision is proposed. The fabric would be energy efficient and minimise energy demands.

Policy EN5 Strategic Areas for low and zero carbon decentralised energy infrastructure The development has a robust energy strategy. There are no plans for district heating or other infrastructure in the local area.

Policy EN6 'Target framework for CO₂ reductions from low or zero carbon energy supplies' - The buildings functions would reduce overall energy demands. The building fabric would be high quality and energy costs should remain low.

Policy EN9 'Green Infrastructure' –The soft landscaping and trees would enhance biodiversity and improve green infrastructure.

Policy EN14 'Flood Risk'- A scheme to minimise surface water runoff would be agreed. The design would not exacerbate existing flood risk and the risk to residents has been minimised.

Policy EN15, 'Biodiversity and Geological Conservation' – The site has low potential for bats and the impact on birds can be mitigated.

Policy EN16 'Air Quality' The impact on air quality would be minimised through careful control of construction activities. Other measures to minimise the impact of the operations of the development include a travel plan and 100% cycle provision.

Policy EN17 'Water Quality' - Water saving measures would minimise surface water runoff. Historic uses means that below ground contamination could impact on ground water. Remediation measures are required to minimise any risk to below ground water quality. Air quality would not be worsened subject to mitigation.

Policy EN18, 'Contaminated Land' – Ground conditions can be addressed. The former use of the site require extensive remediation and conditions would protect ground water and ensure the site is appropriately remediated.

EN19 'Waste' – The waste management strategy incorporates recycling principles.

Policy DM1 'Development Management' - Careful consideration has been given to the design, scale and layout of the buildings along with associated impacts on residential amenity from loss of privacy and daylight and sunlight considerations.

PA1 'Developer Contributions' states that where needs arise as a result of development, the Council will seek to secure planning obligations. A legal agreement would be prepared to secure a mechanism to review the viability at an

appropriate date in the future in order to determine if there has been a change in market conditions to enable a contribution towards affordable housing in the City as required by policy H8.

For the reasons given above, and within the main body of this report, it is considered that the proposal is consistent with the policies contained within the Core Strategy.

The Unitary Development Plan for the City of Manchester (1995)

The Unitary Development Plan for the City of Manchester was adopted in 1995. However, it has now been largely replaced by the Manchester Core Strategy. There are some saved policies which are considered relevant and material and therefore have been given due weight in the consideration of this planning application. The relevant policies are as follows:

Saved Policy DC7 ‘New Housing Developments’ – The proposal represents a high quality accessible development.

Saved policy DC19 ‘Listed Buildings’ – The impact on the listed building is considered in detail in the report.

Saved policy DC20 Archaeology states the Council will give careful consideration to development proposals which affect scheduled Ancient Monuments and sites of archaeological interests, to ensure their preservation in place. This is discussed in detail below.

Saved policy DC26, Development and Noise - The impact from noise sources would be minimised and further mitigation would be secured by planning condition.

For the reasons given below, it is considered that the proposal is consistent with the policies contained within the UDP.

Other material policy considerations

The Guide to Development in Manchester Supplementary Planning Document and Planning Guidance (Adopted 2007)

This document provides guidance to help develop and enhance Manchester. In particular, the SPD seeks appropriate design, quality of public realm, facilities for disabled people (in accordance with Design for Access 2), pedestrians and cyclists. It also promotes a safer environment through Secured by Design principles, appropriate waste management measures and environmental sustainability. Sections of relevance are:

Chapter 2 ‘Design’ – outlines the City Council’s expectations that all new developments should have a high standard of design making a positive contribution to the City’s environment;

Paragraph 2.7 states that encouragement for “the most appropriate form of development to enliven neighbourhoods and sustain local facilities. The layout of the

scheme and the design, scale, massing and orientation of its buildings should achieve a unified form which blends in with, and links to, adjacent areas.

Paragraph 2.8 suggests that in areas of significant change or regeneration, the future role of the area will determine the character and design of both new development and open spaces. It will be important to ensure that the development of new buildings and surrounding landscape relates well to, and helps to enhance, areas that are likely to be retained and contribute to the creation of a positive identity.

Paragraph 2.14 advises that new development should have an appropriate height having regard to the location, character of the area and specific site circumstances. Although a street can successfully accommodate buildings of differing heights, extremes should be avoided unless they provide landmarks of the highest quality and are in appropriate locations.

Paragraph 2.17 states that vistas enable people to locate key buildings and to move confidently between different parts of the neighbourhood or from one area to another. The primary face of buildings should lead the eye along important vistas. Views to important buildings, spaces and landmarks, should be promoted in new developments and enhanced by alterations to existing buildings where the opportunity arises.

Chapter 8 'Community Safety and Crime Prevention' – The aim of this chapter is to ensure that developments design out crime and adopt the standards of Secured by Design;

Chapter 11 'The City's Character Areas' – the aim of this chapter is to ensure that new developments fit comfortably into, and enhance the character of an area of the City, particularly adding to and enhancing the sense of place.

Manchester Residential Quality Guidance (2016)

The City Council's Executive has recently endorsed the Manchester Residential Quality Guidance. As such, the document is now a material planning consideration in the determination of planning applications and weight should be given to this document in decision making.

The purpose of the document is to outline the consideration, qualities and opportunities that will help to deliver high quality residential development as part of successful and sustainable neighbourhoods across Manchester. Above all the guidance seeks to ensure that Manchester can become a City of high quality residential neighbourhood and a place for everyone to live.

The document outlines nine components that combine to deliver high quality residential development, and through safe, inviting neighbourhoods where people want to live. These nine components are as follows:

- Make it Manchester;
- Make it bring people together;
- Make it animate street and spaces;

- Make it easy to get around;
- Make it work with the landscape;
- Make it practical;
- Make it future proof;
- Make it a home; and
- Make it happen.

Manchester Green and Blue Infrastructure Strategy 2015

The Manchester Green and Blue Infrastructure Strategy (G&BIS) sets out objectives for environmental improvements within the City in relation to key objectives for growth and development.

Building on the investment to date in the city's green infrastructure and the understanding of its importance in helping to create a successful city, the vision for green and blue infrastructure in Manchester over the next 10 years is:

By 2025 high quality, well maintained green and blue spaces will be an integral part of all neighbourhoods. The city's communities will be living healthy, fulfilled lives, enjoying access to parks and greenspaces and safe green routes for walking, cycling and exercise throughout the city. Businesses will be investing in areas with a high environmental quality and attractive surroundings, enjoying access to a healthy, talented workforce. New funding models will be in place, ensuring progress achieved by 2025 can be sustained and provide the platform for ongoing investment in the years to follow.

Four objectives have been established to enable the vision to be achieved:

1. Improve the quality and function of existing green and blue infrastructure, to maximise the benefits it delivers
2. Use appropriate green and blue infrastructure as a key component of new developments to help create successful neighbourhoods and support the city's growth
3. Improve connectivity and accessibility to green and blue infrastructure within the city and beyond
4. Improve and promote a wider understanding and awareness of the benefits that green and blue infrastructure provides to residents, the economy and the local environment.

Eastlands Regeneration – A New East Manchester (2008, 2011, 2017)

The Site forms part of the Eastlands Regeneration Framework (“ERF”). The area has been promoted for strategic development since the ‘New East Manchester: A New Town in the City’ was first published. This was eventually superseded by ‘The East Manchester Strategic Regeneration Framework 2008 – 2018’.

Significant new development around Eastlands has either been delivered or is planned, including the recent approval of the game-changing Co-op Live Arena, which will be a world-leading venue delivered by OVG. In addition, Planning Permission for a new world-class workplace campus for small and medium-sized

businesses to form, scale and thrive and set within 1.5 acres of green public realm amenity, has been approved by Manchester City Council earlier this year in relation to a site at Pollard Street.

The ERF provides recommendations for future regeneration activities that will enable new social, community, commercial and development initiatives in and around the Etihad Stadium, with the emphasis on guiding development activity westwards along the Ashton Canal Corridor to connect the Etihad Campus with the established eastward expansion of the city centre, building on its ongoing regeneration momentum.

The application site is located within the Holt Town Central Area as defined within the 2017 ERF update. This area is located between, and connects, the Etihad Campus and the Ancoats and New Islington neighbourhoods. The area has been seen as the 'missing link' in the regeneration of the Ashton Canal Corridor from the City Centre to the Etihad Campus.

The ERF 2017 set out Holt Town as a mixed-use neighbourhood anticipating higher density residential uses including family homes across a range of price points and tenures, employment space for a wide variety of businesses from "makers" to tech entrepreneurs, with local retail and services to support the wider area, making maximum use of the proximity to the Holt Town Metrolink stop.

The ERF also highlights the potential of 'the extraordinary listed former mill buildings' offering significant potential for commercial, residential and cultural uses, creating a neighbourhood of character, similar to Ancoats.

The regeneration of the area will be underpinned by the reuse and preservation of historic mill buildings fronting onto the canal and that infill developments on adjoining sites should be of a scale and character to complement these buildings, reinforcing the respective identities of the old and the new.

City Centre Strategic Plan 2015-2018 (March 2016)

On the 2 March 2016 the City Council's Executive approved the City Centre Strategic Plan which seeks to provide an up-to-date vision for the City Centre within the current economic and strategic context along with outlining the key priorities for the next few years for each City Centre neighbourhood. This document seeks to align itself with the Manchester Strategy (January 2016) along with the Greater Manchester Strategy. Overall the City Centre plan seeks to *"shape the activity that will ensure that the City Centre continues to consolidate its role as a major economic and cultural asset for Greater Manchester and the north of England"*.

It should also be noted that the strategic plan approved by the Executive also endorsed an extended boundary of the City Centre upon which the strategic plan is based. This extended boundary includes the application site and the wider Ancoats and New Islington area.

Manchester Strategy (January 2016)

The strategy sets the long term vision for Manchester's future and how this will be achieved. An important aspect of this strategy is the City Centre and how it will be a key driver of economic growth and a major employment centre. Furthermore, increasing the centre for residential is fundamental along with creating a major visitor destination.

National Planning Policy Framework (2021)

The revised NPPF re-issued in February 2021. The document states that the *'purpose of the planning system is to contribute to the achievement of sustainable development'*. The document clarifies that the *'objective of sustainable development can be summarised as meeting the needs of the present without compromising the ability of future generations to meet their own needs'* (paragraph 7).

In order to achieve sustainable development, the NPPF states that the planning system has three overarching objectives – economic, social and environmental (paragraph 8).

Section 5 *'Delivering a sufficient supply of new homes'* states that *a sufficient amount and variety of land should come forward where it is needed, that the needs of groups with specific housing requirements are addressed and that land with permission is developed without unnecessary delay'* (paragraph 60).

Para 65 states that at least 10% of housing should be for affordable homeownership, unless this would exceed the level of affordable housing required in the area, or significantly prejudice the ability to meet the identified affordable housing needs of specific groups.

This proposal would redevelop a brownfield site and a listed building which is in a poor condition in a key regeneration area for 277 new homes. A mixture of 1, 2 and 3 bedroom accommodation would be provided catering for all family sizes and needs. Viability has been tested and in order to deliver a viable and deliverable scheme to the quality proposed, together with restoring the listed building, the scheme could not support an affordable housing contribution. This is considered in further detail within the report.

Section 6 *'Building a Strong, Competitive Economy'* states that significant weight should be placed on the need to support economic growth and productivity, taking into account both local business needs and wider opportunities for development (paragraph 81).

Accommodation used by local businesses and workspaces would be lost. A relocation package would help those businesses to find alternative accommodation and would be a condition of the planning approval. 2034 sqm of workspaces and commercial space is proposed and may allow some businesses to return.

Section 8 *'Promoting Healthy and Safe Communities'* states that *planning policies and decisions should aim to achieve healthy, inclusive and safe places* (paragraph 92).

The proposal would be safe and secure. Cycle parking is provided along with car parking. Disabled residents would have access to parking. New public realm and green infrastructure would be provided with improved access to the Ashton Canal.

Section 9 '*Promoting Sustainable Transport*' states that '*significant development should be focused on locations which are or can be made sustainable, through limiting the need to travel and offering a genuine choice of transport modes. This can help to reduce congestion and emissions, and improve air quality and public health*' (paragraph 105).

In assessing applications for development, it should be ensured that: appropriate opportunities to promote sustainable transport modes can be – or have been – taken up, given the type of development and its location; safe and suitable access to the site can be achieved for all users; and, the design of streets, parking areas, other transport elements and the content of associated standards reflects national guidance including the National Design Guide and National Model Design Code; any significant impacts from the development on the transport network (in terms of capacity and congestion), or on highway safety, can be cost effectively mitigated to an acceptable degree (paragraph 110).

Developments should only be prevented or refused on highways grounds if there would be an unacceptable impact on highway safety, or the residual cumulative impacts on the road network would be severe (paragraph 111).

Within this context, applications for development should: give priority first to pedestrian and cycle movements, both within the scheme and with neighbouring areas; and second – so far as possible – to facilitating access to high quality public transport, with layouts that maximise the catchment area for bus or other public transport services, and appropriate facilities that encourage public transport use; address the needs of people with disabilities and reduced mobility in relation to all modes of transport; create places that are safe, secure and attractive – which minimise the scope for conflicts between pedestrians, cyclists and vehicles, avoid unnecessary street clutter, and respond to local character and design standards; allow for the efficient delivery of goods, and access by service and emergency vehicles; and, be designed to enable charging of plug-in and other ultra-low emission vehicles in safe, accessible and convenient locations. (paragraph 112)

All developments that generate significant amounts of movement should be required to provide a travel plan, and the application should be supported by a transport statement or transport assessment so that the likely impacts of the proposal can be assessed (paragraph 113).

The site is well connected to all public transport modes which would encourage sustainable travel. There would be no unduly harmful impacts on the traffic network with physical and operational measures to promote non car travel. A travel plan and operational management would be secured as part of the conditions of the approval.

Section 11 '*Making effective use of land*' states that '*planning decisions should promote an effective use of land in meeting the need for homes and other uses, while*

safeguarding and improving the environment and ensuring safe and healthy living conditions' (paragraph 119).

Planning decisions should: encourage multiple benefits from urban land, including through mixed use schemes and taking opportunities to achieve net environmental gains – such as developments that would enable new habitat creation; recognise that some undeveloped land can perform many functions, such as for wildlife, recreation, flood risk mitigation, cooling/shading, carbon storage or food production; give substantial weight to the value of using suitable brownfield land within settlements for identified needs, and support appropriate opportunities to remediate despoiled, degraded, derelict, contaminated or unstable land; promote and support the development of under-utilised land and buildings especially if this would help to meet identified needs for housing where land supply is constrained and available sites could be used more effectively; and, support opportunities to use airspace above existing residential and commercial premises for new homes. (paragraph 120)

Local Planning Authorities should take a positive approach to applications for alternative uses of land which is currently developed but not allocated for a specified purpose in plans, where this would help to meet identified development needs. In particular they should support proposal to: use retail and employment land for homes in areas of high housing demand, provided this would not undermine key economic sectors or site or the vitality and viability of town centres, and would be compatible with other policies in the Framework; make more effective use of sites that provide community services such as schools and hospitals (paragraph 123)

Planning policies and decisions should support development that makes efficient use of land, taking into account: the identified need for different types of housing and other forms of development, and the availability of land suitable for accommodating it; local market conditions and viability; the availability and capacity of infrastructure and services – both existing and proposed – as well as their potential for further improvement and the scope to promote sustainable travel modes that limit future car use; the desirability of maintaining an area's prevailing character and setting (including residential gardens), or of promoting regeneration and change; the important of securing well designed, attractive and healthy spaces (paragraph 124).

Where there is an existing or anticipated shortage of land for meeting identified housing needs, it is especially important that planning decisions avoid homes being built at low densities and ensure that developments make optimal use of the potential of each site. Paragraph 125 (c) states that Local Planning Authorities should refuse applications which they consider fail to make efficient use of land, taking into account the policies in the NPPF. In this context, when considering applications for housing, authorities should take a flexible approach in applying policies or guidance relating to daylight and sunlight, where they would otherwise inhibit making efficient use of a site (as long as the resulting scheme would provide acceptable living standards).

The proposal would re-use a largely vacant listed building and redevelop vacant land. The scale and density of the proposal is considered to be acceptable and represents and efficient use of land. There would be a loss of existing employment uses, but some workspace would be re-provided on site. The 277 homes would help meet known housing and regeneration requirements in East Manchester. The site is close

to sustainable transport infrastructure. A travel plan would encourage the use public transport, walking and cycle routes to the site.

Onsite parking would be provided but the overall objective would be to reduce car journeys. Electric car charging would support a shift away from petrol/diesel cars.

Section 12 '*Achieving Well Designed Places*' states that '*the creation of high quality, beautiful and sustainable buildings and places is fundamental to what the planning and development process should achieve. Good design is a key aspect of sustainable development, creates better places in which to live and work and helps make development acceptable to communities. Being clear about design expectations, and how these will be tested, is essential for achieving this. So too is effective engagement between applicants, communities, local planning authorities and other interest throughout the process*' (paragraph 126).

Planning decisions should ensure that developments: will function well and add to the overall quality of the area, not just for the short term but over the lifetime of the development; are visually attractive as a result of good architecture, layout and appropriate and effective landscaping; are sympathetic to local character and history, including the surrounding built environment and landscape setting, while not preventing or discouraging appropriate innovation or change (such as increased densities); establish or maintain a strong sense of place, using the arrangement of streets, spaces, building types and materials to create attractive, welcoming and distinctive places to live, work and visit; optimise the potential of the site to accommodate and sustain an appropriate amount and mix of development (including green and other public spaces) and support local facilities and transport networks; and create places that are safe, inclusive and accessible and which promote health and well being, with a high standard of amenity for existing and future users and where crime and disorder, and the fear of crime, do not undermine the quality of life or community cohesion and resilience (paragraph 130).

Trees make an important contribution to the character and quality of urban environments and can also help to mitigate and adapt to climate change. Planning decisions should ensure that new streets are tree lined, that opportunities are taken to incorporate trees elsewhere in developments, that appropriate measures are in place to ensure the long term maintenance of newly placed trees and that existing trees are retained wherever possible (paragraph 131).

Development that is not well designed should be refused, specifically where it fails to reflect local design policies and government guidance on design. Conversely, significant weight should be given to: development which reflects local design policies and government guidance on design, taking into account any local design guidance and supplementary planning documents such as design guides and codes; and/or outstanding or innovative design which promote high levels of sustainability, or help raise the standard of design more generally in an area so long as they fit in with the overall form and layout of their surroundings (paragraph 134).

The design would be highly quality and complement the distinctive architecture within the area. The buildings would be sustainable and low carbon. Biodiversity, green infrastructure and water management measures are included within the public realm.

Section 14 '*Meeting the challenge of climate change, flooding and coastal change*' states that the planning system should support the transition to a low carbon future in a changing climate, taking full account of flood risk and coastal change. It should help to: shape places in ways that contribute to radical reductions in greenhouse gas emissions, minimise vulnerability and improve resilience; encourage the reuse of existing resources, including the conversion of existing buildings; and support renewable and low carbon energy and associated infrastructure (paragraph 152).

New development should be planned for in ways that: avoid increased vulnerability to the range of impacts arising from climate change. When new development is brought forward in areas which are vulnerable, care should be taken to ensure that risks can be managed through suitable adaptation measures, including through the planning of green infrastructure; and can help to reduce greenhouse gas emissions, such as through its location orientation and design. Any local requirements for the sustainability of buildings should reflect the Government's policy for national technical standards (paragraph 154).

In determining planning applications, Local Planning Authorities should expect new development to: comply with any development plan policies on local requirements of decentralised energy supply unless it can be demonstrated by the applicant, having regard to the type of development involved and its design, that this is not feasible or viable; and take account of landform, layout, building orientation, massing and landscaping to minimise energy consumption (paragraph 157).

The buildings fabric would be highly efficient and it would predominately use electricity. The landscaping scheme would include trees and planting, Efficient drainage systems would manage water at the site.

Section 15 '*Conserving and Enhancing the natural environment*' states that planning decision should contribute and enhance the natural and local environment by protecting valued landscapes, minimising impacts on and providing net gains for biodiversity, preventing new and existing development from contributing to unacceptable levels of sol, air, water or noise pollution or land instability and remediating contaminated land.

The high performing fabric of the building would ensure no unduly harmful noise outbreak on the local area. Biodiversity improvements would be provided in the form of trees and landscaping which is a significant improvement based on the current condition of the application site.

Paragraph 183 outlines that planning decisions should ensure that a site is suitable for its proposed use taking account of ground conditions and any risks arising from contamination (a).

There is contamination at the site from the former land uses/buildings. The ground conditions are not usual or complex for this part of the city and can be appropriate remediated.

Paragraph 185 outlines that decisions should ensure that ne development is appropriate for its location taking into account the likely effects of pollution in health, living conditions and the natural environment.

There would be some short term noise impacts associated with the construction process but these can be managed to avoid any unduly harmful impacts on amenity. There are not considered to be any noise or lighting implications associated with the operation of the development.

Paragraph 186 states that decisions should sustain and contribute towards compliance with relevant limit values or national objectives for pollutants, taking into account the presence of Air Quality Management Areas and Clean Air Zones. Opportunities to improve air quality or mitigate impacts should be identified, such as through traffic and travel management, and green infrastructure provision and enhancement.

The proposal would not worsen local air quality conditions and suitable mitigation can be put in pace during the construction process. There would be a travel plan and access to public transport for occupants of the development along with 20% of the car parking spaces being fitted with electric vehicle charging points.

Section 16 '*Conserving and enhancing the historic environment*' states that in determining applications, Local Planning authorities should require an applicant to describe the significance of any heritage assets affected, including any contribution made by their setting. The level of detail should be proportionate to the assets' importance and no more than is sufficient to understand the potential impact of the proposal on their significance. As a minimum the relevant historic environment record should have been consulted and the heritage assets assessed using appropriate expertise where necessary. Where a site on which development is proposed includes, or has the potential to include, heritage assets with archaeological interest, local planning authorities should require developers to submit an appropriate desk-based assessment and, where necessary, a field evaluation (paragraph 194).

In determining applications, local planning authorities should take account of: the desirability of sustaining and enhancing the significance of heritage assets and putting them to viable uses consistent with their conservation; b) the positive contribution that conservation of heritage assets can make to sustainable communities including their economic vitality; and c) the desirability of new development making a positive contribution to local character and distinctiveness. (Paragraph 197)

In considering the impacts of proposals, paragraph 199 states that the impact of a proposal on the significance of a designated heritage asset, great weight should be given to the asset's conservation (and the more important the asset, the greater the weight should be). This is irrespective of whether any potential harm amounts to substantial harm, total loss or less than substantial harm to its significance.

Paragraph 200 goes on to state that any harm to, or loss of, the significance of a designated heritage asset (from its alteration or destruction, or from development within its setting), should require clear and convincing justification.

Paragraph 202 states that where a development proposal will lead to less than substantial harm to the significance of a designated heritage asset, this harm should be weighed against the public benefits of the proposal including, where appropriate, securing its optimum viable use.

The effect of an application on the significance of a non-designated heritage asset should be taken into account in determining the application. In weighing applications that directly or indirectly affect non-designated heritage assets, a balanced judgement will be required having regard to the scale of any harm or loss and the significance of the heritage asset (paragraph 203).

The proposal would not result in a degree of harm to the heritage assets. This is considered in detail in the report.

Paragraphs 10, 11, 12, 13 and 14 of the NPPF outline a “presumption in favour of sustainable development”. This means approving development, without delay, where it accords with the development plan and where the development is absent or relevant policies are out-of-date, to grant planning permission unless any adverse impacts of doing so would significantly and demonstrably outweigh the benefits when assessed against the NPPF.

Planning Policy Guidance (PPG)

The relevant sections of the PPG are as follows:

Air Quality provides guidance on how this should be considered for new developments. Paragraph 8 states that mitigation options where necessary will be locationally specific, will depend on the proposed development and should be proportionate to the likely impact. It is important therefore that local planning authorities work with applicants to consider appropriate mitigation so as to ensure the new development is appropriate for its location and unacceptable risks are prevented. Planning conditions and obligations can be used to secure mitigation where the relevant tests are met.

Examples of mitigation include:

- the design and layout of development to increase separation distances from sources of air pollution;
- using green infrastructure, in particular trees, to absorb dust and other pollutants;
- means of ventilation;
- promoting infrastructure to promote modes of transport with low impact on air quality;
- controlling dust and emissions from construction, operation and demolition;
- and

- contributing funding to measures, including those identified in air quality action plans and low emission strategies, designed to offset the impact on air quality arising from new development.

Noise states that Local planning authorities' should take account of the acoustic environment and in doing so consider:

- whether or not a significant adverse effect is occurring or likely to occur;
- whether or not an adverse effect is occurring or likely to occur; and
- whether or not a good standard of amenity can be achieved.

Mitigating the noise impacts of a development will depend on the type of development being considered and the character of the proposed location. In general, for noise making developments, there are four broad types of mitigation:

- engineering: reducing the noise generated at source and/or containing the noise generated;
- layout: where possible, optimising the distance between the source and noise-sensitive receptors and/or incorporating good design to minimise noise transmission through the use of screening by natural or purpose built barriers, or other buildings;
- using planning conditions/obligations to restrict activities allowed on the site at certain times and/or specifying permissible noise levels differentiating as appropriate between different times of day, such as evenings and late at night, and;
- mitigating the impact on areas likely to be affected by noise including through noise insulation when the impact is on a building.

Design states that where appropriate the following should be considered:

- layout – the way in which buildings and spaces relate to each other
- form – the shape of buildings
- scale – the size of buildings
- detailing – the important smaller elements of building and spaces
- materials – what a building is made from

Health and well being states opportunities for healthy lifestyles have been considered (e.g. planning for an environment that supports people of all ages in making healthy choices, helps to promote active travel and physical activity, and promotes access to healthier food, high quality open spaces and opportunities for play, sport and recreation);

Travel Plans, Transport Assessments in decision taking states that applications can positively contribute to:

- encouraging sustainable travel;
- lessening traffic generation and its detrimental impacts;
- reducing carbon emissions and climate impacts;
- creating accessible, connected, inclusive communities;
- improving health outcomes and quality of life;

- improving road safety; and
- reducing the need for new development to increase existing road capacity or provide new roads.

Heritage states that Public benefits may follow from many developments and could be anything that delivers economic, social or environmental objectives as described in the National Planning Policy Framework (paragraph 8). Public benefits should flow from the Proposed Development. They should be of a nature or scale to be of benefit to the public at large and not just be a private benefit. However, benefits do not always have to be visible or accessible to the public in order to be genuine public benefits, for example, works to a listed private dwelling which secure its future as a designated heritage asset could be a public benefit.”

Public benefits may also include heritage benefits, such as:

- Sustaining or enhancing the significance of a heritage asset and the contribution of its setting;
- Reducing or removing risks to a heritage asset;
- Securing the optimum viable use of a heritage asset in support of its long-term conservation.

Other legislative requirements

Section 16 (2) of the Planning (Listed Building and Conservation Areas) Act 1990 (the "Listed Building Act") provides that "in considering whether to grant listed building consent for any works to a listed building, the local planning authority or the Secretary of State shall have special regard to the desirability of preserving the building or its setting or any features of special architectural or historic interest which it possesses"

Section 66 Listed Building Act requires the local planning authority to have special regard to the desirability of preserving the setting of listed buildings. This requires more than a simple balancing exercise and case law has considerable importance and weight should be given to any impact upon a designated heritage asset but in particular upon the desirability of preserving the setting with a strong presumption to preserve the asset.

S149 (Public Sector Equality Duty) of the Equality Act 2010 requires due regard to the need to: Eliminate unlawful discrimination, harassment and victimisation and other conduct prohibited by the Act and; Advance equality of opportunity between persons who share a protected characteristic and persons who do not share it. The Equality Duty does not impose a legal requirement to conduct an Equality Impact Assessment. Compliance with the Equality Duty involves consciously thinking about the aims of the Equality Duty as part of the process of decision-making.

Environmental Impact Assessment

The Town and Country Planning (Environmental Impact Assessment) Regulations 2017 specifies that certain types of development require an Environmental Impact Assessment (EIA) to be undertaken.

The nature of the proposal falls exceeds of the threshold within “Urban Development Projects” which is 150 residential units. A Screening Opinion has been adopted which confirms that the environmental effects of this development are not significant to warrant an EIA.

Heritage Significance

Brunswick Mill (Grade II) is an surviving example of a former cotton spinning mill, which is a dominant presence within its surrounding streetscene. This is the result not only of its scale, but also of the quality of its architectural detailing, including decorative string courses and an imposing stone archway around the entranceway. These give the complex considerable visual interest.

The mill is also of particular historic interest, not least because the majority of the buildings date to a single phase of construction. The complex is therefore an example of holistic site planning, and a complete example of a mid-nineteenth century fireproof mill. The mill also has later phases of evolution, which are of considerable interest in their own right. Of these, the buildings and infrastructure associated with the mill’s conversion to electrical power are of particular interest, as it was the first mill in Manchester where this occurred.

More widely, mills formed an integral part of Manchester’s nineteenth century evolution into an industrial titan, and are one of the defining features of the city.

Together they have considerable historic value, both as part of the evolution of Manchester, and as part of the Industrial Revolution, a chain of events that fundamentally shaped the nation.

Pooley’s (later India) Mill. This mill dated to the early nineteenth century, but was demolished following a fire in the mid-2000s.

Issues

Principle of the redevelopment of the site and contribution to regeneration

Regeneration is an important planning consideration. There is a crucial link between economic growth, regeneration and the provision of new homes and as the economy of the city recovers post-pandemic, more are required to fuel and complement it.

Manchester’s population has increased rapidly since 2001 and is expected to increase considerably by 2030. This, together with trends and changes in household formation, requires additional housing. Around 3,000 new homes are required each year and the proposal would contribute to this need. Providing the right quality and diversity of new housing for the increasing population is critical to continued growth and success.

East Manchester has been identified for high density housing and this development would deliver a variety of housing types and be attractive to families. The proposal

would be a key development between the city centre and the Etihad Campus and restore and repair a listed building that is in poor state of repair.

227 homes would be delivered in a highly sustainable, well-connected location with enhanced public realm. The homes and commercial activity would bring increased footfall and activity and complement regeneration in New Islington, Miles Platting and around the Etihad Campus. There would be one, two and three-bed apartments and townhouses and would be suitable to families. The sizes would be consistent with the City's space standards with all one bed apartments suitable for 2 people. There would be a significant amount of workspace, retail and community space.

There are 11 active tenants who are on short term leases and are important local businesses. The proposal includes a relocation package to allow them to remain active, preferably within East Manchester. Confirmation of the relocation package would be a condition.

There would be workspace and commercial uses at ground and part of first floor in the Mill with homes in the rest of the building. Two buildings would be developed at the adjacent site between 5 and 8 storeys. Active ground floor, entrances and the townhouses would animate Bradford Road.

This proposal would form an important catalyst in the regeneration of East Manchester and re-purpose one of the last remaining significant mill buildings in the area. The proposal would help to realise the visions set out in the various development frameworks as underpinned by policy SP1 of the Core Strategy.

Significant economic and social benefits include the creation of approximately 152 construction jobs for the duration of the construction and 224 indirect jobs. The GVA associated with these jobs would be £24.2 million per year. The commercial elements would create 156.5 jobs with annual GVA of £5.8m per year. An estimated 88 jobs and GVA of £1.67m would be lost from the site based on its current occupation. Therefore, the net jobs created is 68.5 with annual GVA of £4.1m. The new households would spend around £5 million per year. 277 new homes would generate Council Tax revenue of around £0.5 million per annum.

A local labour agreement would ensure that these economic and social benefits are fully realised.

These socio-economic benefit are significant and would remove an underutilised site and support economic and population growth which would create jobs and increase local spending and taxation.

It is considered that the development would be consistent with the regeneration frameworks for this area including the City Centre Strategic Plan and would complement and build upon the City Council's current and planned regeneration initiatives. The proposal is therefore considered to be consistent with sections 1 and 2 of the National Planning Policy Framework, and Core Strategy policies H1, H4, SP1, EC1, EC3, CC1, CC3, CC4, CC7, CC8, CC10, EN1 and DM1. As such, it is necessary to consider the potential impact of the development.

Affordable Housing

Policy H8 establishes that new development should contribute to the City-wide target for 20% of new housing being affordable and 20% should be used as a starting point for calculating affordable housing provision. Developers should provide new homes that are available for social or affordable rent or affordable home ownership, or provide an equivalent financial contribution.

The amount of affordable housing should reflect the type and size of development as a whole and should take into account factors such as an assessment of local need, any requirement to diversify housing mix and the need to deliver other key outcomes, particularly regeneration objectives.

An applicant may seek an exemption from providing affordable housing, or a lower proportion of affordable housing, a variation in the mix of affordable housing, or a lower commuted sum, should a viability assessment demonstrate that a scheme could only deliver a proportion of the 20% target; or where material considerations indicate that intermediate or social rented housing would be inappropriate. Examples of these circumstances are set out in part 4 of Policy H8.

The application proposes 277 homes for open market sale and 2034 sqm of work space, retail and community uses (Use Class E and Use Class F2). The delivery of homes and the regeneration of East Manchester is a key Council priority.

The proposal would develop a contaminated, brownfield site containing a grade II listed building which is in a poor state of repair. The condition of the listed building and the vacant nature of the wider application site has a negative impact on the character and appearance of the area.

This high quality development proposes the sensitive alteration, repair and conversion of the listed building. All accommodation would comply with the Residential Quality guide, active frontages would enliven Bradford Road and Beswick Street with new public realm and improvement to access along the Ashton Canal. All these matters have an impact on viability.

A viability report, which has been made publicly available through the Councils public access system has been independently assessed on behalf of the Council. This concludes that the scheme would not be viable if it was to support an affordable housing contribution. A benchmark land value of £1,480,080 is within the expected range based on comparable evidence. The Gross Development Value would be £59,787,800 which would give a profit of 18% on GDV.

On this basis, the scheme could not support an affordable housing contribution. This would ensure that the scheme is viable and can be delivered to the quality proposed.

The viability would be subject to review at an agreed future date to determine any uplift in market conditions which may improve the viability and secure a contribution towards affordable housing in line with the requirements of policy H8.

Climate change, sustainability and energy efficiency

The proposal would be a low carbon building in a highly sustainable location with all forms of public transport nearby.

Sustainability principles would be incorporated into the construction process to minimise and recycle waste, ensure efficiency in vehicle movements and sourcing and use of materials.

There would be 81 car parking spaces with 20% fitted with an electric charging point. The remainder would have infrastructure and could be adapted in the future. A travel plan would encourage residents to use public transport to reduce vehicle trips. A secure cycle store would provide 100% provision.

The all electric building would benefit as the grid decarbonises. The fabric would be highly efficient to prevent heat loss together with energy saving fixtures and fittings such as LED lighting and a mechanical ventilation system. The heating and cooling systems would be highly efficient. Photovoltaic panels to the roof would generate renewable energy.

The development would achieve a 9.10% improvement on Part L (2013) in line with the requirements of policy EN6 which seeks to achieve a 15% reduction in CO2 on Part L (2010 (or 9% over Part L 2013) Building Regulations. A condition requiring a post construction review would verify that this reduction has been achieved.

The landscaping, trees (including street trees) would deliver enhance biodiversity and an efficient drainage system would minimise the effects of surface water.

Impact of the historic environment and cultural heritage

Brunswick Mill is Grade II listed. It is a former Victorian Cotton Mill and the first in Manchester to be powered by electricity. The site is not in a conservation area. There are listed buildings in the local area which could be affected by the development. These are the Ashton Canal Bridge No.5 (Grade II), Spectator Mill (Grade II), Hope Mill (Grade II*) and the Ashton Canal Bridge No.4 (Grade II).

The applicant has provided a heritage statement, a detailed design and access statement and structural report which examines current condition and impact of the proposal on the significance of the listed building, its setting and the impact on surrounding listed buildings.

The building contains many original features which, despite the overall condition of the building, remain relatively intact. It was constructed of plain red brick, with higher quality soft red/orange brick forming the pilasters. The main entrance off Bradford Road has rusticated stone dressings.



Main entrance off Bradford Road

There are several outbuildings; the engine house to the south west corner and the waste house in the opposite corner. These were original outbuildings and illustrate original functions and operations of the site. There are later buildings in the courtyard. Two 1880s to the north east and north west corners and a 1908 electric transformer station to the south west corner.



View from within the courtyard including transformer building (left)

The mill was constructed using fire proof methods, consisting of shallow brick arched floors supported with cast iron columns and brick walls. Internally, the mill is open plan with cast iron columns supported by brick Jack arches with iron reinforcement tie rods. Some floors have been subdivided with partition walls.



Typical floor layout showing columns and original terracotta quarry tiles

At the fourth floor level of the east wing is the highly decorative canteen which was fitted within green and cream glazed tiling around 1919. The staircases have a striking bull nosed termination at the 7th floor and use triangular stone steps supported by a central brick wall. The original slender iron handrail remains evident within the walls of the staircase.



Staircase including steps, handrail and bull nose

The courtyard contains original cobbles and cast iron fire escapes.



Cast iron fire escapes

The most highly significant elements are the main four wings of the building, its cobbled courtyard, engine and waste house and two original stone staircases. These are largely unaltered and are judged to be significant in understanding the role of the site as a cotton mill.

Considerable significance is given to the canteen tiling to the third floor of the east wing associated with the staff canteen together with the 1920s addition of a top floor level to the north entrance block and waste house. The courtyard fire escapes also fall within this category along with the terracotta tiling to the mill floors and the remaining surviving casement windows. The transformer station is significant due to its role and significance with the mill being the first to be powered by electricity.

Features which are judged to be of low significance are the 1880 loading bays within the courtyard and the 1950s buildings to the east of the site.

The setting of the site, adjacent to the Ashton Canal, contributes to the significance of the mill allowing its former industrial use to be understood. In today's context it feels relatively isolated, being one of the last remaining mill buildings in the area. The vacant elements of the site have a negative impact on the setting of the listed building.

The refurbishment and conversion of the mill requires repair, alteration, removal and demolition at the complex and a conservation led approach has been taken

All existing rainwater goods would be replaced with replica cast iron goods which would restore the exterior of the building and ensure it is protected from water ingress. The impact of this work is considered to be beneficial.

Windows and window openings would be replaced with powder coated aluminium casements to the same 19th Century six pane design. The removal of listed fabric would cause a degree of harm. However, the condition of the windows precludes their retention and a scheme to replicate their design and aesthetic would be acceptable in this instance.

The condition of the masonry varies and would be cleaned and pointed with lime mortar. Missing or deteriorated brick work, stonework and terracotta vents would be replaced. Surviving stucco to the upper parts of the engine house would be repaired and replaced. The impact of this work would be beneficial. The modern security gates to the main entrance would be removed and replaced and this would be beneficial.

Various re-roofing works would be required to the north entrance block, main mill building and former engine house. The impact of this work is judged to be neutral.

The 1920s extension to the 3rd floor roof of the north entrance block, the 1950s extension to the north east corner of the building (and replace with new boundary treatment to screen newly created car parking), the modern extension to the north west corner of the mill, the later brick extension to west elevation of the former engine house together with the later courtyard extensions and the 1960s brick lift tower to east elevation would be demolished. The lack of significance of these more modern elements is judged to be moderately beneficial to the significance of the listed building.

The fire escapes in the internal courtyard would also be removed, however, the brackets would be retained as evidence of their presence. This would have some moderate adverse impact on the significance of this listed building but the retention of the brackets would retain some legibility to the fire strategy of the mill.

Various interventions would be required in the courtyard including the creation of new entrance to the north courtyard elevation and relaying of the cobbles. These would have a minor adverse impact on the significance of the listed building. The transformer station would be retained and converted into a commercial unit.

New openings to the southern and Bradford Road elevations are required to increase pedestrian movements from the canal towpath. This would reuse openings which are currently blocked with a neutral/moderately beneficial impact.

More significant interventions are required internally to facilitate the conversion of the listed building such as new vertical risers, new lifts, new staircase to north entrance block, raised access floors, internal partitions to create the commercial units and apartments, new concierge reception, modification to the commercial and residential waste store. These elements would cause a minor/moderate adverse impact on the significance of the listed building principally from the loss of openness within the internal spaces which would prevent the special character of the former mill floors to

be appreciated and would only be reversed if partitions and corridors were to be removed.

These floors contain many original features such as original staircases, Jack arches, exposed internal perimeter walls, columns, internal structural beams and structural ties which would be repaired and exposed within the commercial or residential accommodation. This would allow many original features to remain within the listed building and appreciated. The Edwardian glazed tiles of the former canteen would also be incorporated as features walls within the third-floor apartments. This has a moderate beneficial impact on the significance of the listed building.

The works to the listed building are extensive in order to convert it into a commercial and residential space. This would cause harm to the listed building and its significance which would only be reversed if modern interventions are removed. The most notable are the subdivision of the large open plan floor plates which are part of the special character of former mill buildings.

Whilst there is be an element of openness retained at the ground level in the commercial units, there would overall be a degree of harm. There would, however, be an extensive programme of repair and restoration works which largely seeks to retain existing features in situ.

Consideration has been given to the impact of the mid and corner buildings on the setting of the listed building and other listed buildings in the surrounding areas. The heritage statement has considered 6 viewpoints to understand the visual impact of the development of the identified heritage assets.

View point 1 is from the junction of Beswick Street and Pollard Street. The grade II listed bridge No. 5 can be seen to the left although it is best appreciated from the canal towpath. The altered remains of the former mill building of Wellington Mills can be seen with the top of Brunswick Mill.

Viewpoint 01 Existing



Viewpoint 01 Proposed



This view does not fully represent significance of Brunswick Mill but does highlight the industrial character of the area albeit in a fragmented form. The new buildings would be visible and the images highlight that a modern coherent form of development would be created for the setting of Brunswick Mill.

Viewpoint 2 from the north side of Old Mill Street to the west of the junction with Bradford Road/ Beswick Street and is dominated by road traffic and low rise housing together with the Mill which appears isolated due to the erosion of its historic context.

Viewpoint 02 existing



Viewpoint 02 proposed



The view demonstrates that the proposal would re-instate the historic urban form along the southern side of Bradford Road and enhance the setting of the listed mill.

Viewpoint 3 is a kinetic view from view 2 from across the road junction. It provides a closer view of the sense of dereliction and fragmentation of the urban form in the south eastern part of the site and highlights the sense of isolation of the listed building from the loss of its historical context.

Viewpoint 03 existing



Viewpoint 03 proposed



The proposed view highlights the positive impact the new buildings on the setting of the listed building, re-establishing the building line along Bradford Road terminating by the angular corner block which returns along Beswick Street.

View 4 looks north east along the Ashton Canal from the grade II listed bridge No. 4 on Carruthers Street. Hope Mill (Grade II*) and Spectator Mill (Grade II) provide a historical context to the right hand side of the view in contrast to the left which is more modern. The improvements to the canal towpath provide an enhanced long ranging view of Brunswick Mill with collective value with the other older buildings.

Viewpoint 04 existing



Viewpoint 04 proposed



The proposed development would not be visible from this viewpoint.

View 5 is from the bridge over the Ashton Canal at Cambrian Street looking south west and is another vantage point from which to appreciate the Mill.

Viewpoint 05 existing



Viewpoint 05 proposed



The proposed view highlight the positive impact its restoration and the new building would not be visible.

View 6 is from the junction of Bradford Road and Clayton Street looking south and highlights the condition of the building and its boarded up windows.

Viewpoint 06 existing



Viewpoint 06 proposed



The proposed view demonstrates that impact the restoration of the mill would have. Windows would be re-opened and fabric repaired. There is a glimpsed view of the proposed middle building which appears as a subservient element to the listed mill.

The overall impact of the development on surrounding listed building is judged to be moderate beneficial to negligible beneficial.

The works would result in *less than substantial harm* to the grade II listed building and therefore the test set out in paragraph 202 of the NPPF apply.

Historic England have raised no objections to the conversion of the mill but have expressed some concern about the extent of subdivision, exposure of features and demolition of the transformer building. The transformer building would now be retained and features such as columns fully exposed. Historic England consider the new build within its setting requires further consideration.

The conversion of the listed building would cause harm to its significance. The new build would be large and significant buildings in its setting. The most highly significant features of the listed building would be repaired and restored with modern and less significant interventions removed. The scale, massing, appearance and use of materials would provide a complementary development along Bradford Road and Beswick Street re-establishing built form.

The proposal would result in a low level of *less than substantial harm* as defined by paragraph 202 of the NPPF, to the significance and setting of the Grade II listed building. As directed by paragraph 202 of the NPPF, it is now necessary to consider whether the public benefits required exist which outweighs any this harm. These public benefits will be considered in detail below.

Assessment of Heritage Impact

The proposal would create instances of less than substantial harm as defined within the NPPF. Any level of harm should be outweighed by the public benefits that would be delivered in accordance with the guidance provided in paragraph 202 of the NPPF. In assessing the public benefits, consideration has been given to paragraph 8 of the NPPF which outlines the three dimensions to achieve sustainable development: economic, social and environmental.

The redevelopment and regeneration of this brownfield site is in line with Council policy and would bring 277 homes in a highly sustainable area.

The key views demonstrate how the development would have a beneficial impact on the majority of views and setting of the listed building through its use of materials, position back of footpath re-establishing development on the Bradford Road/Beswick Street junction.

Whilst the building would be large, it would not be out of context with other older buildings which line the canal towpath. The listed building would remain legible and understood due to the gaps created between the mill and the mid building.

Trees and resurfacing along Bradford Road and enhanced public realm and pedestrian links to the canal towpath would increase natural surveillance and permeability and improve biodiversity.

The proposal create 152 full time equivalent jobs during construction and 214 jobs in the supply chain (assuming a 1.5 year build programme). The GVA associated with these jobs would be £24.2 million per yard.

The homes could accommodate up to 637 residents within 449 additional working age residents and an additional £5 million of expenditure to the local economy.

The new workspaces and commercial space at the development would create 156.5 jobs in total with annual GVA of £5.8 million per year.

The new build would be a low carbon and the mill would be as energy efficient as possible. An all electric system would benefit from a decarbonising grid. Photovoltaic panels to the roof would generate on site energy 20% of the parking spaces would be fitted with electric car charging points and the remainder adaptable in future. 100% cycle provision would be available.

The significant public benefits would outweigh the heritage impacts which would be at the lower end of less than substantial harm.

It is considered, therefore, that, notwithstanding the considerable weight that must be given to preserving the setting of the listed buildings as required by virtue of S66 of the Listed Buildings Act. The harm caused would be less than substantial and would be outweighed by the public benefits of the scheme and meet the requirements set out in paragraph 202 of the NPPF.

Impact on Archaeology

There is archaeological interest relating to the Pooley's Mill. Greater Manchester Archaeology Advisory Service (GMAAS) consider that further investigations are required prior to the commencement of any ground works. They also recommend that any demolition works are subject to a building survey. This would satisfy the requirements of policy EN3 of the Core Strategy and saved UDP policy DC20.

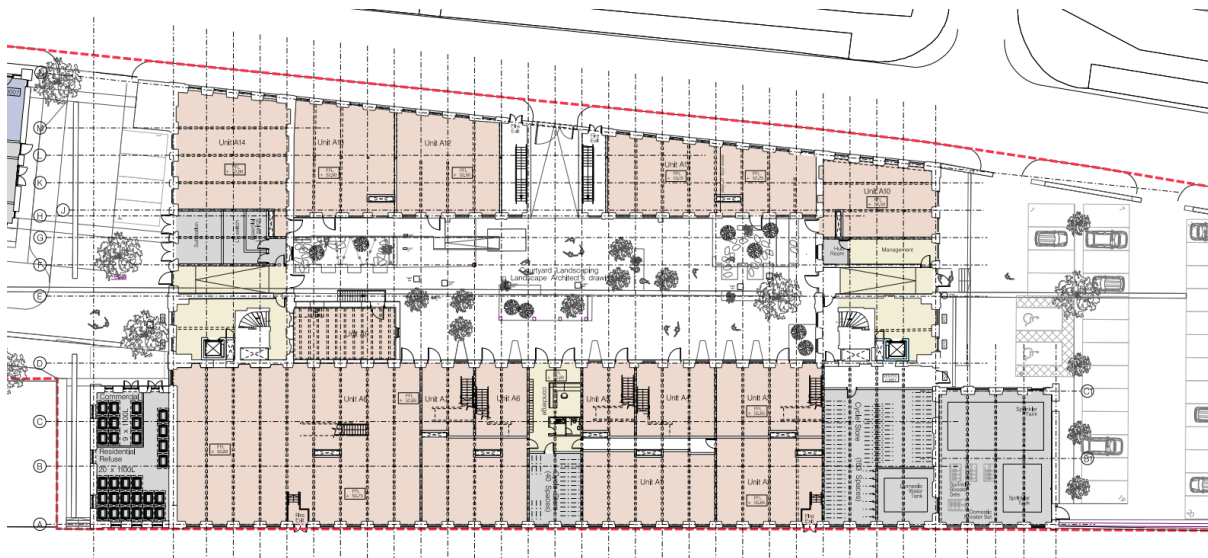
Layout, scale, external appearance and visual amenity

Works to the listed building involve an extensive programme of repair, re-instatement and alterations. This would include new windows, selective demolition, repair to masonry and re-purposing external areas of car parking and amenity space.

The courtyard would be transformed into a landscaped space with amenity space for residents, businesses and visitors. The courtyard would act as a circulation space linking the residential cores of the building which are at either side of the mill.

The ground floor would be divided into work space/commercial units and the Jack arches would be exposed. Units facing the courtyard are accessed from it through

door openings created in window openings. The units facing the canal are accessed from the concierge spaces.



Ground floor layout of the Mill Building

The former Engine House would become a bin store and the former waste house a cycle store and water tank. The remaining buildings within this space would be demolished and a 32 space car park created.

The boundary wall to Bradford Road would be re-built using salvaged and re-claimed bricks. Two gated vehicular entrances would provide access from this car park.

Level one of the mill is partially occupied by the upper floors of the commercial unit that face the south eastern side of the courtyard. The remainder of the floor forms apartments. Apartments are replicated up to level 6. Each floor has two circulation cores providing lift and stair access.

The subdivision and internal partitions to each apartment follows the Jack arches. This allows the masonry structure to the floor above to be exposed. All pipe work and other services would be hidden within a raised section of floor instead of affecting the ceilings.

The mid-block would be positioned fronting Bradford Road and responds to the building line of the mill, re-establishing built form on this vacant section of Bradford Road which was once dominated by a mill building.

A new vehicular and pedestrian access off Bradford Road would provide separation between the listed building and the new building and allow the listed building to remain legible and understood. Public realm, a new link to the canal and a 49 space car park is located to the rear of the building.

Townhouses front Bradford Road with living accommodation and front doors which help activate the street. Four townhouses also front the car parking area again with the own front door.



Layout of the mid and corner buildings

At the first floor of the mid building, are the bedroom accommodation of the townhouses along with a series of apartments. Apartments are replicated up to the 5th floor. At the sixth floor, the accommodation steps back to provide communal roof gardens and a series of private terraces.

The above plan shows that there is land to the rear of the mid building, and in between the mid and corner buildings, which falls outside of the application site. Feasibility studies have demonstrated that development at this site would not preclude development of this land in the future.

The corner building would wrap around the corner of Bradford Road and Beswick Street providing a prominent development in this position and would again follow the established building line along Bradford Road. A commercial unit would be located at the ground floor with a pedestrian entrance from Beswick Street. The pedestrian entrance to the upper floor residential accommodation would be located on Bradford Road. The upper floors of the building contain the apartments.

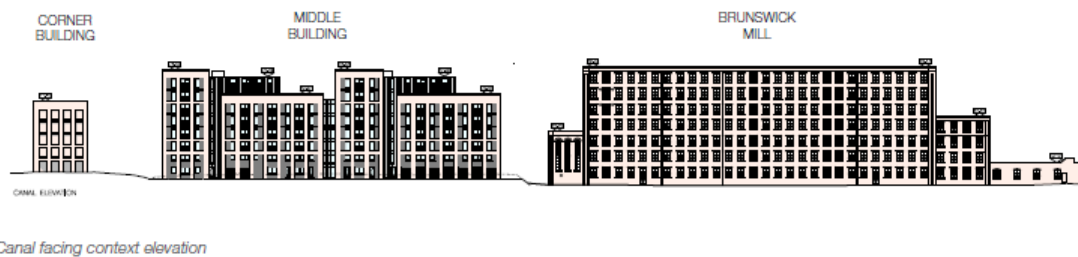
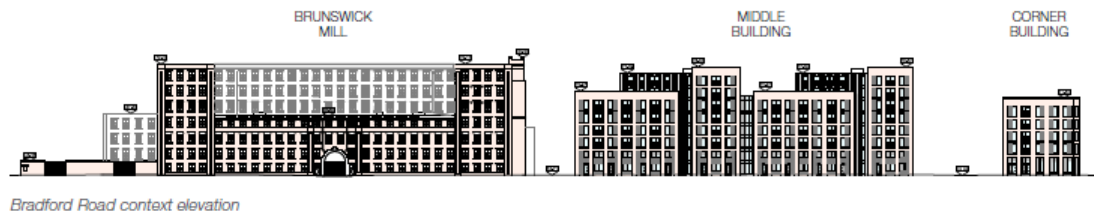
The layout of the development is considered to be acceptable. There would be a degree of separation between the new building and the listed building. The new buildings would re-establish the building line along this section of Bradford Road terminating at the corner building which would wrap around to Beswick Street. Active frontages would be created by townhouses and a commercial unit. Areas of car parking would be minimised behind the buildings or proposed boundary treatment.

Improved access to the canal towpath would also be created along with new areas of public realm.

The scale and height of the new buildings would retain the dominance of the listed mill the street scene. The mid building would be lower than the Mill.



Image of the restored mill and mid building



Street scene along Bradford Road and the Ashton Canal

The massing has been fragmented with recessed elements infilled with standing seam zinc to contrast with the masonry elevations. This provides the overall effect of two buildings. A glazed recessed link joints the elements together.



Image showing change on massing and height of the mid building

The corner block is lower in height and relates to the lower of the masonry elements of the mid-block. The overall appearance when viewing the new build elements within the context of the mill is of a series of lower horizontal masonry blocks.

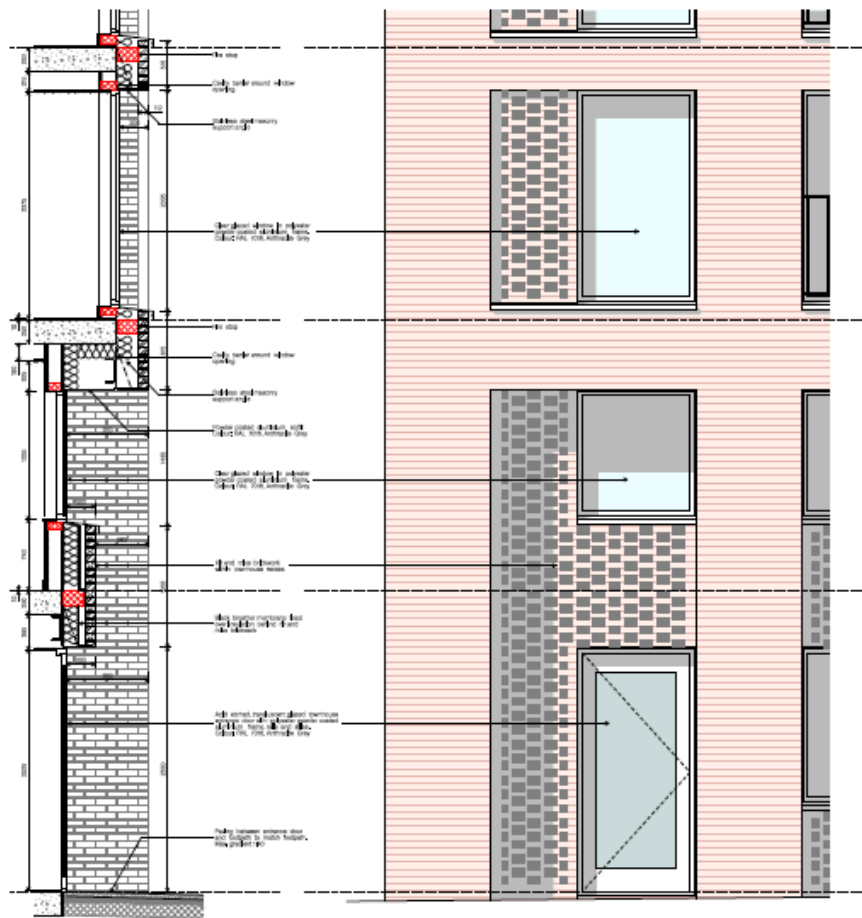


Image of the corner building and mid building

The new build would be a large development in the context of the listed building and the local area. On the opposite side of Bradford Road, are low rise 2-3 storey residential properties. However, the wider context comprises large former industrial buildings. There would be no unacceptable impacts on residential amenity from the development. The scale is considered appropriate given the historical use of the site and the context of the listed building. The new buildings would help redefine the street scene. The appearance of the mid and corner building draw upon the aesthetics of listed mill with the regularity of the fenestration and robust masonry façade.

The main façade of the mid and corner buildings is a red tone brick with a regular window pattern. Deep, recessed windows with hit and miss panels provide simple and effective architectural detailing to the elevations. The hit and miss panels also allow for the ventilation elements of the apartments to be concealed.

The townhouses at ground level are recessed further still, which acts as a buffer, providing a deeper threshold between the public street and the private space of the dwelling behind the facade.



Mid-block, Facade Fragment - Bradford Road Elevation

Bay study of the window arrangements

Zinc cladding at the 6 and 7 floor, which are set back from the main façade, provide a subservient element to the more dominant masonry facades.



Image at the corner of Bradford Road and Beswick Street

The layout and scale would complement the character, scale and order of development in the surrounding area whilst retaining and repairing the listed building.

The design of the new buildings would form distinctive pieces of architecture in the setting of the listed building. The materials deliver a simple and effective façade treatment. Conditions of the planning approval will ensure that the materials are appropriate and undertaken to the highest standard.

Contribution to Improving Permeability, Public Spaces and Facilities and Provision of a Well Designed Environment

The landscape and public realm strategy focuses on three main areas:

- Space to the north east of the existing mill (formerly occupied by one and two storey ancillary buildings);
- The courtyard of the existing mill; and
- The space between the proposed new building blocks and the neighbouring land to the rear adjacent to the Ashton Canal.

A space between the existing mill and the middle building would create a sense of arrival. This would provide the parking access a pedestrian link from Bradford Road through the site to the mill, and its ground floor commercial spaces and the Ashton Canal Towpath. An amenity area for sitting would be provided with the pedestrian route residents, workers, visitors and towpath users.

A west-east pedestrian link would be provided from the middle building car park through the mill and courtyard to the north east car park. This would be highlighted with a common palette of materials and create permeability.

The landscaping scheme uses reclaimed cobbles, semi-mature trees and feature banding which directs pedestrian to entrances and minimise the visual affect of the car parking and vehicular access in the setting of the listed building. A green gap paving solution would be adopted and further softened by the use of planting, hedging and trees. Sustainable drainage would allow permeable drainage of surface water. The same design principles are applied to the parking area to the north east of the mill.

The mill courtyard would redeveloped to ensure that it is usable and enhances the setting of the listed building. The original cobbled courtyard would be re-laid. The level differences in the courtyard would need a ramp deck to provide level access to the commercial/workspaces. Pockets of hard and soft landscaping are proposed in the former loading bays and their position defined by their retained low walls. These spaces would be available to residents, visitors and workers.

The landscaping/public realm strategy would contribute positively to the Mill and complement its character and setting. The high quality materials and mature planting and trees would create inviting spaces.

The linkages across the site and between buildings and the access to the Canal Towpath would increase permeability. The enhanced natural surveillance on the walking and cycling route from the development is considered a key benefit of this scheme. Measure to improve green infrastructure, biodiversity and minimise the impacts of surface water enhance the quality of this scheme and minimise its impact on the local environment and climate change.

The final specification of the hard and soft landscaping scheme would be agreed by planning condition.

Impact on Ecology

An ecological appraisal assesses the potential impact of the development on local ecology and nature conservation. This is a key requirement of policies EN15 and DM1 which seeks to ensure that applicants identify, enhance and restore impacts on local habitats.

Impact on bat activity at the site has been considered including submission of a method statement to undertake work at the site whilst minimise impact on bats. Greater Manchester Ecology Unit concur with the statement submitted but advise confirmation is provided that relevant licenses are in place. In addition, given the proximity to the Ashton Canal, a method statement should be agreed to minimise the impacts from construction activities on this area.

Biodiversity measures should be agreed in line with the recommendations of the ecology report and this should be a condition of the planning approval.

Effects on the Local Environment/ Amenity

(a) Sunlight, daylight, overshadowing and overlooking

An assessment has been undertaken to establish the likely effects on daylight and sun light received by properties around the site. Consideration has also been given to instances of overlooking which would result in a loss of privacy.

The BRE guidelines have been used to provide a method for assessing daylight – Vertical Sky Component (VSC) and No Sky Line (NSL) methods. For sunlight, the approach considers the Annual Probable Sunlight Hours (APSH) for a reference point on a window i.e. if a window point can receive at least 25% APSH, then the room should still receive enough sunlight.

The following properties were assessed as part of the survey:

- 166 Old Mill Street;
- 35, 37, 39, 41, 43, 45, 47, 49, 51, 53, 55, 57, 59, 61, 63, 65, 67 and 69 Bradford Road;
- 8, 10, 12, 14, 16, 18, 20 and 22 Ridgeway Street;
- 1, 3, 5, 7, 9, 11 and 13 Halmore Road;
- Land to the south side of Ridgeway Street;
- 155, 157, 159, 161 and 163 Butler Street;
- New developments by Lovells at Butler Street, Bradford Road and Halmore Road.

Consideration should be given to paragraph 123 (c) of section 11 of the NPPF which states that when considering applications for housing, a flexible approach should be taken in terms of applying policies or guidance relating to daylight and sunlight, where they would otherwise inhibit making efficient use of a site; as long as the resulting scheme would provide acceptable living standards.

The results show 81% compliance for daylight with regard to Vertical Sky Component (the amount of light reaching a window) and a further 8% that would only experience minor adverse effect. 91% of rooms would pass the BRE criteria for daylight distribution within those rooms with a further 2% experiencing a minor adverse effect.

A proportion of the windows and rooms affected are bedrooms which the BRE guide states should be treated as less important than other habitable rooms and other windows would in reality retain good levels of absolute VSC (the measure of light reaching a window).

There would be 100% compliance for sunlight in summer and winter.

Therefore, the proposal would have no unduly harmful impact on the daylight and sunlight available to affected property that would warrant refusal. The proposal is separated from existing properties by the road network which provides adequate separation distances to prevent loss privacy from overlooking.

(b) TV reception

A TV reception survey has concluded that there would be minimal impact on digital television services or digital satellite television services. This would be closely

monitored during the works and a condition would require a post completion survey to be undertaken to verify this and that no additional mitigation is required.

(c) Air Quality

An air quality report notes that the site is located in the Greater Manchester Air Quality Management Area (AQMA) (where there are exceedance of annual mean nitrogen dioxide). Roads in the AQMA are likely to be used by traffic associated with construction and the completed development. The site is partially vacant and any increase in activity would be noticeable.

There are homes, businesses, primary schools and recreational areas which could be affected by construction traffic and activities and when the development is complete and all are highly sensitive to air quality impacts.

The main contributors to air quality conditions would be from construction from dust, particulate matter and pollution concentrations generated on site, particularly from exhaust emissions from traffic, plant and earthworks. Nearby homes are therefore likely to experience the effects of this during the construction period. There would be emissions from construction traffic which will enter the site via Bradford Road.

The report concludes that the impact on human health would be low and would be minimised by dust suppression measures and other good practices which must be implemented throughout the construction period and would be secured as part of the construction management plan condition.

When the development is occupied, the effect on local air quality conditions would not be significant with only minor localised impacts which are within an acceptable range. 20% of the 81 parking spaces would be fitted with an electric car charging points with the remaining being adaptable at a later stage. There is 100% cycle provision and a travel plan would support sustainable travel choices and promote excellent public transport links.

Environmental Health concur with the conclusions and recommendations in the air quality report. The mitigation measures would be secured by planning condition and the proposal would comply with policy EN16 of the Core Strategy, paragraph 8 of the PPG and paragraph 124 of the NPPF in that there would be no detrimental impact on existing air quality conditions as a result of the development.

Noise and vibration

A noise assessment identifies the main sources during construction would be from plant, equipment and general construction activities, including breaking ground and servicing. Noise levels from construction would be acceptable provided the strict operating and delivery hours are adhered to along with the provision of an acoustic site hoarding, equipment silencers and regular communication with residents. This should be secured by a condition.

When the development is occupied, the acoustic specification of the homes would limit noise ingress from external noise, particularly nearby roads. This would be the

verified prior to occupation. Acoustic insulation would be required to the commercial and workspace accommodation to prevent unacceptable noise transfer.

Provided that construction activities are carefully controlled and the plant equipment and apartments are appropriately insulated the proposal would be in accordance with policy DM1 of the Core Strategy, extant policy DC26 of the UDP and the NPPF

Waste management

Each apartment would have storage for refuse, recyclable and compostable materials in the kitchen and utility area. Separate compartments would be provided for each waste stream. Residents would be responsible for taking waste to the bin storage areas on the ground floor of each building. The total amount of waste storage across the 3 buildings would be in line with the City Council waste guidance: Mill Building: 71.25 sqm; Mid Building: 42.18 sqm; and, Corner Building: 26.49 sqm.

The commercial uses would store their waste in dedicated stores, adjacent to the residential stores. The total commercial waste storage area is: Mill Building: 26.14 sqm; and, Corner Building: 14.78 sqm.

The waste would be collected from a loading bay in the main car park. The collection point for the Corner Building is on Beswick Street. The refuse vehicle can use the collection point without interfering with the operation of Beswick Street and the junction signals. The waste arrangements would be managed by the onsite facilities management team and serviced by Council collections. Commercial waste would be collected by a private contractor. Given the anticipated low volume of waste, it would be collected weekly.

Environmental Health consider the waste arrangements to be acceptable and in line with City Council waste guidance for high rise residential developments.

Accessibility

The proposal would create an inclusive environment for all. All entrances would be level. The public realm would be level. There is lift access to all parts of the listed building and new buildings. The townhouses would meet the City Council's space standards and can be adapted further to meet individual occupiers needs. 5% of the car parking at the development would be allocated for disabled users.

Flood Risk/surface drainage

The site is in flood zone 1 'low probability of flooding' and in a critical drainage area where there are complex surface water flooding problems from ordinary watercourses, culverts and flooding from the sewer network. These areas are particularly sensitive to an increase in surface water run off and/or volume from new developments which may exasperate local flooding problems. Policy EN14 requires development to minimise its impact on surface water run off in critical drainage area.

A drainage statement has been considered by the City Council's flood risk management team. Further details are required to complete the drainage strategy in

order to satisfy the provision of policy EN14 of the Core Strategy which should form part of the conditions of the planning approval.

Impact on the highway network/car/cycle parking and servicing

A transport statement notes that all sustainable transport modes are nearby including Holt Town and New Islington tram stops. The transport assessment indicates that the proposal would have a minimal impact on the highway network.

The 81 parking spaces (26%), including 7 bays for disabled people, would be split across two parking areas. 20% of the bays would be fitted with a fast charging electric car charging point with the rest of the bays being adaptable at a later stage.

There would be 317 secure cycle spaces. A travel plan would support residents travel needs. A condition should ensure that the travel plan is monitored and that residents are supported to find a parking space should they require it.

The main access and servicing routes would be off Bradford Road with provision created off Beswick Street. The servicing arrangements are satisfactory.

Highway measures required for pedestrian and highway safety include upgrading pedestrian crossing, amendments to existing traffic regulation orders, installation of a car club bay on street and upgrading of the pedestrian footways including provision of tactile paving. A construction management plan is also required to be agreed.

The proposal therefore accords with policies SP1, T1, T2 and DM1 of the Core Strategy.

Designing out crime

A Crime Impact Statement (CIS), prepared by Design for Security at Greater Manchester Police, recognises that the development would bring vitality to this area and more active frontage. It is recommended that a condition of the planning approval is that the CIS is implemented in full to achieve Secured by Design Accreditation.

Ground conditions

A ground conditions report details that the site is contaminated from previous uses and requires remediation prior to redevelopment. The ground conditions are not complex so as to prevent development provided a strategy is prepared, implemented and the works verified. This approach should form a condition of the planning approval in order to comply with policy EN18 of the Core Strategy.

Construction management

The work would take place close to existing homes and comings and goings from the site are likely to be noticeable. There has been a significant amount of development activity in the area for a number of years which has caused some localised disruption to residents.

The construction impacts would be temporary and predictable allowing them to be appropriately mitigated. A condition would require approval of a construction management plan which would include details of dust suppression measures, highways management plan and details of machinery use. Wheel washing would prevent any dirt and debris along the road. This would ensure that any cumulative impacts associated with other development can be managed to ensure no unduly harmful impacts on residents and business.

Construction vehicles are likely to use Old Mill Street/Bradford Road via Great Ancoats Street or Alan Turing Way. The use of these major road routes would minimise disruption on more minor roads although it is acknowledged that this section of Bradford Road is lined with homes who would experience vehicle movements for the duration of the construction programme.

Provided the initiatives outlined above are adhered to, it is considered that the construction activities are in accordance with policies SP1 and DM1 of the Core Strategy and extant policy DC26 of the Unitary Development Plan. However, it is recommended that a condition of the planning approval is that the final construction management plan is agreed in order to ensuring the process has the minimal impact on surrounding residents and the highway network.

Permitted Development

The National Planning Policy Guidance states that only in exceptional circumstances should conditions be imposed which restrict permitted development rights otherwise such conditions are deemed to be unreasonable.

It is recommended that the permitted development rights that would normally allow the change of use of a property to a HMO falling within use classes C3(b) and C3(c) be restricted and that a condition be attached to this effect. This is important given the emphasis and need for family housing in the city.

It is also considered appropriate to remove the right to extend the new building apartment building upwards and remove boundary treatments without express planning permission as these would, it is envisaged, could undermine the design quality of the scheme and in respect of boundary treatment, remove important and high quality features from the street scene.

Public Opinion

Comments have been received which express concern about the scale and height of the new buildings within the setting of the listed building together with the loss of accommodation for local businesses, lack of affordable housing, impacts associated with construction work and loss of light to low rise residential properties on the opposite side of Bradford Road.

The proposal is in line with the East Manchester Regeneration Framework Update and it has been demonstrated that the scale, mass and appearance is appropriate in its context including the setting of the listed building.

Whilst this would be a significant development, the impact on the local context would largely be beneficial and would not result in any unduly harmful impact on residential amenity that would warrant refusal of this planning application.

The impact on the existing businesses in the mill has been a key consideration. A relocation package would support those business in finding alternative, and preferable local, premises. The development would provide 2034 sqm of workspace/commercial floorspace for business to return to the site. This relocation strategy would be condition of the planning approval.

Construction management issues would be carefully considered and a strategy agreed through the planning conditions. This would include ensuring a communication strategy is in place with residents and businesses to minimise disruption around the local highway network.

Matters relating to affordable housing are considered in detail above.

Legal Agreement

A legal agreement under section 106 of the Planning Act would secure a mechanism to re-test the viability of the scheme at an agreed future date to determine if there has been a change in conditions which would enable an affordable housing contribution to be secured in line with policy H8 of the Core Strategy as explained in the paragraph with heading “Affordable housing”.

Conclusion

The proposal conforms to the development plan taken as a whole as directed by section 38 (6) of the Planning and Compulsory Purchase Act 2004 and there are no material considerations which would indicate otherwise.

The site is in an important regeneration area where change and development is expected to take place in line with Council regeneration frameworks (policies SP1 and EC3). The Eastland Framework identifies development in this area for up to 8 storeys and for a mixed use residential led scheme to meet expected housing an population growth in the area.

This proposal would contribute to the supply of new homes and provide 277 one, two and 3 bedroom apartments along with commercial accommodation. Active frontages and high quality façades would make a positive contribution to the street scene by restoring this listed building and removing a vacant site from its setting. The development would be of a high level of sustainability and high quality materials thereby reducing CO2 emissions.

Careful consideration has been given to the impact of the development on the local area (including residential properties, business, schools and recreational areas) and it has been demonstrated that there would be no unduly harmful impacts on noise, traffic generation, air quality, water management, contamination or loss of daylight

and sunlight. Where harm does arise, it can be appropriately mitigated, and would not amount to a reason to refuse this planning application.

Construction impacts can also be appropriately mitigated to minimise the effect on the local residents and businesses.

The proposal would amount to less than substantial harm to the listed building but this is significantly outweighed by the substantial public benefits which would be delivered as a consequence of the development (socially, economically and environmentally as required by S66 meeting the criteria in paragraph 202 of the NPPF in full. There is a clear and convincing justification for the proposal.

Human Rights Act 1998 considerations – This application needs to be considered against the provisions of the Human Rights Act 1998. Under Article 6, the applicants (and those third parties, including local residents, who have made representations) have the right to a fair hearing and to this end the Committee must give full consideration to their comments.

Protocol 1 Article 1, and Article 8 where appropriate, confer(s) a right of respect for a person's home, other land and business assets. In taking account of all material considerations, including Council policy as set out in the Core Strategy and saved policies of the Unitary Development Plan, the Director of Planning, Building Control & Licensing has concluded that some rights conferred by these articles on the applicant(s)/objector(s)/resident(s) and other occupiers and owners of nearby land that might be affected may be interfered with but that that interference is in accordance with the law and justified by being in the public interest and on the basis of the planning merits of the development proposal. She believes that any restriction on these rights posed by the of the application is proportionate to the wider benefits of and that such a decision falls within the margin of discretion afforded to the Council under the Town and Country Planning Acts.

Planning Application 130394/FO/2021

Recommendation Minded to Approve subject to the signing of a legal agreement to secure a re-testing of the viability to determine whether a future affordable housing contribution can be secured.

Article 35 Declaration

Officers have worked with the applicant in a positive and proactive manner based on seeking solutions to problems arising in relation to dealing with the planning application. Pre application advice has been sought in respect of this matter where early discussions took place regarding the siting/layout, scale, design and appearance of the development along with noise, traffic and air quality impacts. Further work and discussion have taken place with the applicant through the course of the application. The proposal is considered to be acceptable and therefore determined within a timely manner.

Condition(s) to be attached to decision for approval

1) The development must be begun not later than the expiration of three years beginning with the date of this permission.

Reason - Required to be imposed pursuant to Section 91 of the Town and Country Planning Act 1990.

2) The development hereby approved shall be carried out in accordance with the following drawings and documents:

Drawings

L (--) 002 Rev P6, L (--) 200 Rev P8, L (--) 2002 Rev P6, L (--) 231 Rev P3, L (--) 232 Rev P3, L (--) 233 Rev P3, L (--) 234 Rev P3, L (--) 436 Rev P4, L (--) 438 Rev P3, L (--) 510 Rev P4, L (--) 511 Rev P2, L (SK) 026, L (SK) 027 and 106-LYR-XX-XX-M2D-L-0001-02 Rev 1 stamped as received by the City Council, as Local Planning Authority, on the 19 August 2021.

L (--) 202 Rev P7, L (--) 222 Rev P6 and 232 Rev P3 stamped as received by the City Council, as Local Planning Authority, on the 18 August 2021

846-04-CIV-XX-XX-SK-C-0001 Rev P01, 846-04-CIV-XX-XX-GA-C-0001 Rev P01, 846-04-CIV-XX-XX-GA-C-0002 Rev P01, 846-04-CIV-XX-XX-GA-C-0003 Rev P01, existing 2 yr run off rates and greenfield run off rates stamped as received by the City Council, as Local Planning Authority, on the 4 August 2021

L(--)100 Rev P6, L(--)101 Rev P5, L(--)102 Rev P5, L(--)103 Rev P5, L(--)104 Rev P5, L(--)105 Rev P5, L(--)106 Rev P5, L(--)107 Rev P5, L(--)108 Rev P5, L(--)203 Rev P5, L(--)204 Rev P5, L(--)205 Rev P5, L(--)206 Rev P5, L(--)207 Rev P5, L(--)220 Rev P5, L(--)221 Rev P5, L(--)222 Rev P5, L(--)223 Rev P4, L(--)224 Rev P4, L(--)225 Rev P4, L(--)226 Rev P4, L(--)227 Rev P4, L(--)400 Rev P3, L(--)410 Rev P3, L(--)415 Rev P1, L(--)420 Rev P3, L(--)430 Rev P2, L(--)431 Rev P2, L(--)432 Rev P2, L(--)433 Rev P2, L(--)435 Rev P2, L(--)437 Rev P3, L(--)439 Rev P2, L(--)470 Rev P2, L(--)471 Rev P1, L (--) 900 Rev P1, L(--)901 Rev P1, , L(--)902 Rev P1, L(--)910 Rev P2, L(--)915 Rev P1, L(--)916 Rev P1, L(--)920 Rev P2, L(--)921 Rev P1, , L(--)922 Rev P1 and L(--)B01 Rev P1 stamped as received by the City Council, as Local Planning Authority, on the 13 May 2021

Supporting Information

Method statement for Bats stamped as received by the City Council, as Local Planning Authority, on the 19 August 2021

Crime Impact Review June V1 and Bat emergence and transect survey stamped as received by the City Council, as Local Planning Authority, on the 4 August 2021

Email from John Cooper regarding Drainage dated 2 August 2021

Design and Access Statement by Hodder and Partners, Landscape and Public Realm Strategy by Layer, Daylight and Sunlight Report by AA Projects, Heritage

Assessment by Stephen Levrant Heritage Architecture, Window Condition Report by Stephen Levrant Heritage Architecture, Archaeological Assessment by Salford University, Crime Impact Statement by Wardell Armstrong, Statement of Consultation by Deloitte Real Estate, Travel Plan by Curtins, Transport Statement by Curtins, Waste Management Strategy by Curtins, Ecological Survey by Rachel Hacking Associates, Energy Statement and ESS by Clancy, Ventilation Report by Clancy, Utilities Report by Clancy, Broadband Assessment by Clancy, Flood Risk and Drainage Statement by Civic, Noise Statement by Azymuth Acoustics UK, Management Strategy by JLL, Air Quality Statement by Ensaf, TV Reception Survey by G-Tech, Desk Top Geo-Environmental Survey by LK Consulting, Construction Methodology Statement by Civic, Structural Statement by Civic, Feasibility Report by Civic, Local Labour Agreement by Maryland Securities Ltd and Viability Report by Cushman and Wakefield stamped as received by the City Council, as Local Planning Authority, on the 13 May 2021

Reason - To ensure that the development is carried out in accordance with the approved plans. Pursuant to policies SP1 and DM1 of the Core Strategy.

3) Prior to the commencement of works to Brunswick Mill, a relocation strategy for the commercial uses shall be submitted for approval in writing by the City Council, as Local Planning Authority. This shall include details of the support and assistance given to relocate the existing businesses and timescales for relocation. The approved strategy shall then be implemented and confirmation provided as to the effectiveness of the strategy within a timescale to be agreed in writing with the City Council, as Local Planning Authority.

Reason - To ensure that a strategy is in place to relocate the existing office use from the premises pursuant to policies SP1, EC1 and EC2 of the Manchester Core Strategy (2012).

4) Prior to the commencement of the development, details of the method for piling, or any other foundation design using penetrative methods, for the development shall be submitted for approval in writing by the City Council, as Local Planning Authority. The approved details shall then be implemented during the construction of the development.

Reason - Piling or any other foundation using penetrative methods can result in risks to potable supplies (pollution/turbidity, risk of mobilising contamination) drilling through different aquifers and creating preferential pathways. It is therefore necessary to demonstrate that piling will not result in contamination of groundwater. In addition, piling can affect the adjacent railway network which also requires consideration pursuant to policies SP1, EN17 and EN18 of the Manchester Core Strategy (2012).

5) No demolition works or vegetation clearance shall take place during the optimum period for bird nesting (March - September inclusive) unless nesting birds have been shown to be absent, or, a method statement for the demolition including for the protection of any nesting birds is agreed in writing by the City Council, Local Planning Authority. Any method statement shall then be implemented for the duration of the demolition works.

Reason - In order to protect wildlife from works that may impact on their habitats pursuant to policy EN15 of the Manchester Core Strategy (2012).

6) Notwithstanding drawings 846-04-CIV-XX-XX-SK-C-0001 Rev P01, 846-04-CIV-XX-XX-GA-C-0001 Rev P01, 846-04-CIV-XX-XX-GA-C-0002 Rev P01, 846-04-CIV-XX-XX-GA-C-0003 Rev P01, existing 2 yr run off rates and greenfield run off rates stamped as received by the City Council, as Local Planning Authority, on the 4 August 2021 and the drainage statement by civic stamped as received by the City Council, as Local Planning Authority, on the 13 May 2021, (a) the development shall not commence until a scheme for the drainage of surface water from that phase of the new development shall be submitted for approval in writing by the City Council as the Local Planning Authority. This shall include:

- Details of how surface water run-off will be attenuated on site and not enter the public highway (specifically on access road of the 'mid building');
- Details of how the surface water run-off from roof areas will drain and be attenuated on site;
- The hydraulic model for the 'existing mill' does not correlate with the proposed drainage layout, clarification needed;
- Assessment of overland flow routes for extreme events that is diverted away from buildings (including basements). Overland flow routes need to be designed to convey the flood water in a safe manner in the event of a blockage or exceedance of the proposed drainage system capacity including inlet structures. A layout with overland flow routes needs to be presented with appreciation of these overland flow routes with regards to the properties on site and adjacent properties off site. Flow routes are not shown onsite for the 'Mid Building'
- Correspondence from United Utilities stating that the 1200mm diameter surface water sewer within Beswick Street is a surface water asset owned by United Utilities and not an ordinary watercourse. The UU correspondence confirms a maximum pass flow of 5l/s for the 'new development area', whereas the proposed drainage layout shows a discharge rate totalling 10l/s (two sperate flow controls of 5l/s) for the 'new development' are confirmation from UU needed that this is acceptable.
- A CCTV survey should be undertaken of the existing combined water sewer and surface water sewer to confirm a gravity connection is feasible.
- Construction details of Aco Channel

(b) The development shall then be constructed in accordance with the approved details, within an agreed timescale.

Reason - To promote sustainable development, secure proper drainage and to manage the risk of flooding and pollution pursuant to policies SP1, EN14 and DM1 of the Manchester Core Strategy (2012).

The above conditions is required as it is essential that an adequate drainage system is designed for the development.

7) No demolition or development shall take place until the applicant or their agents or successors in title has secured the implementation of a programme of archaeological works. The works are to be undertaken in accordance with a Written Scheme of Investigation (WSI) submitted for approval in writing by the City Council, as Local Planning Authority. The WSI shall cover the following:

1 Informed by the updated North West Regional Research Framework, a phased programme and methodology of investigation and recording to include:

- a historic building investigation (Level 2/3) of any historic buildings or structures that are to be removed
- archaeological evaluation trenching;
- pending the results of the above, targeted open-area excavation.

2. A programme for post-investigation assessment to include:

- production of a final report on the results of the investigations and their significance.

3. Deposition of the final report with the Greater Manchester Historic Environment Record.

4. Dissemination of the results of the archaeological investigations commensurate with their significance, including provision for a permanent information panel.

5. Provision for archive deposition of the report and records of the site investigation.

6. Nomination of a competent person or persons/organisation to undertake the works set out within the approved WSI.

Reason: To record and advance understanding of heritage assets impacted on by the development and to make information about the heritage interest publicly accessible pursuant to policies EN3 of the Manchester Core Strategy (2012) and saved policy DC20 of the Unitary Development Plan for the City of Manchester (1995).

8) a) Notwithstanding Phase 1 Preliminary Risk Assessment, Brunswick Mill, Bradford Road, LK Consult, Reference: LKC 18 1200, Dated: June 2018 stamped as received by the City Council, as Local Planning Authority, on the 13 May 2021, the development shall not commence until the following information has been submitted for approval in writing by the City Council, as Local Planning Authority, to identify and evaluate all potential sources and impacts of any ground contamination, groundwater contamination and/or ground gas relevant to the site

- Submission of Site Investigation Proposals;
- Submission of a Site Investigation and Risk Assessment Report; and
- Submission of a Remediation Strategy

b) When the development commences, the development shall be carried out in accordance with the previously agreed Remediation Strategy and a Completion/Verification Report shall be submitted to and approved in writing by the

City Council as Local Planning Authority, prior to the first occupation of the residential element of the development.

In the event that ground contamination, groundwater contamination and/or ground gas, not previously identified, are found to be present on the site at any time before the development is occupied, then development shall cease and/or the development shall not be occupied until, a report outlining what measures, if any, are required to remediate the land (the Revised Remediation Strategy) is submitted to and approved in writing by the City Council as Local Planning Authority and the development shall be carried out in accordance with the Revised Remediation Strategy, which shall take precedence over any Remediation Strategy or earlier Revised Remediation Strategy.

Reason - To ensure that the presence of or the potential for any contaminated land and/or groundwater is detected and appropriate remedial action is taken in the interests of public safety, pursuant to policies DM1 and EN18 of the Core Strategy.

9) The development shall not commence until a detailed construction management plan outlining working practices during construction have be submitted for approval in writing by the Local Planning Authority, which for the avoidance of doubt should include;

- Display of an emergency contact number;
- Details of Wheel Washing;
- Dust suppression measures;
- Compound locations where relevant;
- Consultation with local residents/local businesses;
- Location, removal and recycling of waste;
- Routing strategy and swept path analysis;
- Parking of construction vehicles and staff; and
- Sheeting over of construction vehicles.

Manchester City Council encourages all contractors to be 'considerate contractors' when working in the city by being aware of the needs of neighbours and the environment. Membership of the Considerate Constructors Scheme is highly recommended.

The development shall be carried out in accordance with the approved construction management plan.

Reason - To safeguard the amenities of nearby residents and highway safety, pursuant to policies SP1, EN9, EN19 and DM1 of the Manchester Core Strategy (July 2012).

10) The development (including demolition, ground works, vegetation clearance) shall not commence until a Biodiversity Construction Management has been submitted for approval in writing by the City Council, as Local Planning Authority. The Plan shall include the following.

- Risk assessment of potentially damaging construction activities.
- Identification of "biodiversity protection zones";

- Practical measures (both physical measures and sensitive working practices) to avoid or reduce impacts during construction (may be provided as a set of method statements);
- The location and timing of sensitive works to avoid harm to biodiversity features;
- The times during construction when specialist ecologists need to be present on site to oversee works;
- Responsible persons and lines of communication;
- The role and responsibilities on site of an ecological clerk of works (ECoW) or similarly competent person;
- Use of protective fences, exclusion barriers and warning signs.

The approved Plan shall implemented throughout the duration of the construction period.

Reason - To safeguard the adjacent Ashton Canal pursuant to policies SP1, EN9, EN15 and DM1 of the Manchester Core Strategy (July 2012).

11) Prior to the commencement of the development, all material to be used on all external elevations of the development shall be submitted for approval in writing by the City Council, as Local Planning Authority. This shall include the submission of samples (including a panel) and specifications of all materials to be used on all external elevations of the development along with jointing and fixing details, window reveals and soffits, details of the drips to be used to prevent staining in, ventilation/air brick and a strategy for quality control management.

The approved materials shall then be implemented as part of the development.

Reason - To ensure that the appearance of the development is acceptable to the City Council as local planning authority in the interests of the visual amenity of the area within which the site is located, as specified in policies SP1 and DM1 of the Core Strategy.

12) The window reveals and soffits to the mid and corner building of the development shall be carried out in accordance with drawings L(--)415 stamped as received by the City Council, as Local Planning Authority, on the 13 May 2021.

Reason – In the interest of preserving the architectural detailing on the scheme pursuant to policies EN1 and DM1 of the Manchester Core Strategy (2012).

13) a) Prior to the commencement of the development, details of a Local Benefit Proposal, in order to demonstrate commitment to recruit local labour for the duration of the construction of the development, shall be submitted for approval in writing by the City Council, as Local Planning Authority. The approved document shall be implemented as part of the construction of the development.

In this condition a Local Benefit Proposal means a document which includes:

- i) the measures proposed to recruit local people including apprenticeships
- ii) mechanisms for the implementation and delivery of the Local Benefit Proposal

iii) measures to monitor and review the effectiveness of the Local Benefit Proposal in achieving the objective of recruiting and supporting local labour objectives

(b) Within one month prior to construction work associated with the development being completed, a detailed report which takes into account the information and outcomes about local labour recruitment pursuant to items (i) and (ii) above shall be submitted for approval in writing by the City Council as Local Planning Authority.

Reason – The applicant has demonstrated a commitment to recruiting local labour pursuant to policies SP1, EC1 and DM1 of the Manchester Core Strategy (2012).

14) Prior to any above ground works, details of all the boundary treatment and access gates to Bradford Road shall for the development shall be submitted for approval in writing by the Council, as Local Planning Authority. The approved details shall then be implemented and be in place prior to the first occupation of the development.

The boundary treatment shall be retained and maintained in situ thereafter and notwithstanding the provisions of the Town and Country Planning (General Permitted Development) Order 2015 (or any order revoking or re-enacting that Order with or without modification) no boundary treatment shall be erected on site, other than that shown on the approved plans.

Reason - To ensure that the appearance of the development is acceptable to the City Council as local planning authority in the interests of the visual amenity of the area within which the site is located, as specified in policies SP1 and DM1 of the Core Strategy.

15) Prior to the first occupation of the development hereby approved, details of the implementation, maintenance and management of the sustainable drainage scheme for the development shall be submitted for approval in writing by the City Council, as Local Planning Authority.

For the avoidance of doubt the scheme shall include the following:

- Verification report providing photographic evidence of construction; and
- Management and maintenance plan for the lifetime of the development which shall include the arrangements for adoption by any public body or statutory undertaker, or any other arrangements to secure the operation of the sustainable drainage scheme throughout its lifetime.

The approved scheme shall then be implemented in accordance with the details and thereafter managed and maintained for as long as the development remains in use.

Reason - To promote sustainable development, secure proper drainage and to manage the risk of flooding and pollution pursuant to policies SP1, EN14 and DM1 of the Manchester Core Strategy (2012).

16) The development hereby approved shall be carried out in accordance with the Environmental Standards and Energy Statement by Clancy Consulting stamped as received by the City Council, as Local Planning Authority, on the 13 May 2021. A post construction review certificate/statement for the development shall be submitted for approval, within a timescale that has been previously agreed in writing, to the City Council as Local Planning Authority.

Reason - In order to minimise the environmental impact of the development pursuant to policies SP1, T1-T3, EN4-EN7 and DM1 of the Core Strategy and the principles contained within The Guide to Development in Manchester SPD (2007) and the National Planning Policy Framework.

17) Notwithstanding drawings 106-LYR-XX-XX-M2D-L-0001-02 Rev 1 stamped as received by the City Council, as Local Planning Authority, on the 19 August 2021, (a) prior to any above ground works details of the hard and soft landscaping scheme (including appropriate materials, specifications) shall be submitted for approval in writing by the City Council as Local Planning Authority.

(b) The approved scheme shall be implemented prior to the first occupation of the residential element of the development. If within a period of 5 years from the date of the planting of any tree or shrub, that tree or shrub or any tree or shrub planted in replacement for it, is removed, uprooted or destroyed or dies, or becomes, in the opinion of the local Planning Authority, seriously damaged or defective, another tree or shrub of the same species and size as that originally planted shall be planted at the same place.

Reason - To ensure that a satisfactory landscaping scheme for the development is carried out that respects the character and visual amenities of the area, in accordance with policies SP1, EN9 and DM1 of the Core Strategy.

18) (a) Prior to the first occupation of the development, details of any externally mounted ancillary plant, equipment and servicing shall be submitted for approval in writing by the City Council, as Local Planning Authority. For the avoidance of doubt, externally mounted plant, equipment and servicing shall be selected and/or acoustically treated in accordance with a scheme designed so as to achieve a rating level of 5 db (L_{aeq}) below the typical background (L_{a90}) level at the nearest noise sensitive location.

(b) Prior to the first occupation of the development, a verification report will be required to validate that the work undertaken conforms to the recommendations and requirements approved as part of part (a) of this planning condition. The verification report shall include post completion testing to confirm the noise criteria has been met. In instances of non-conformity, these shall be detailed along with mitigation measures required to ensure compliance with the noise criteria. A verification report and measures shall be agreed until such a time as the development complies with part (a) of this planning condition.

Any mitigation measures shall be implemented in accordance with a timescale to be agreed with the City Council, as Local Planning Authority. Any measures shall thereafter retained and maintained in situ.

Reason - To minimise the impact of plant on the occupants of the development pursuant to policies SP1 and DM1 of the Manchester Core Strategy (2012) and saved policy DC26 of the Unitary Development Plan for the City of Manchester (1995).

19) (a) Notwithstanding the Noise Statement by Azymuth Acoustics Uk stamped as received by the City Council, as Local Planning Authority, on the 13 May 2021, prior to the first use of each commercial/workspace unit as indicated on drawings L (--) Rev P8, L (--) 231 Rev P3 and L (--) 100 Rev P6 stamped as received by the City Council, as Local Planning Authority, on the 19 August and 13 May 2021 respectively, a scheme of acoustic insulation for the commercial/workspace units shall be submitted for approval in writing by the City Council, as Local Planning Authority.

(b) Prior to the first use of each of the commercial units, a verification report will be required to validate that the work undertaken conforms to the recommendations and requirements approved as part of part (a) of this planning condition. The verification report shall include post completion testing to confirm the noise criteria has been met. In instances of non conformity, these shall be detailed along with mitigation measures required to ensure compliance with the noise criteria. A verification report and measures shall be agreed until such a time as the development complies with part (a) of this planning condition.

Any mitigation measures shall be implemented in accordance with a timescale to be agreed with the City Council, as Local Planning Authority, and thereafter retained and maintained in situ.

Reason - In order to limit the outbreak of noise from the commercial premises pursuant to policies SP1 and DM1 of the Core Strategy (2007) and saved policy DC26 of the Unitary Development Plan for the City of Manchester (1995).

20) Notwithstanding the Noise Statement by Azymuth Acoustics Uk stamped as received by the City Council, as Local Planning Authority, on the 13 May 2021, prior to the first occupation of the development a scheme for acoustically insulating the proposed residential accommodation against noise from the local traffic network shall be submitted for approval in writing by the City Council, as Local Planning Authority. The approved noise insulation scheme shall be completed before the first occupation of the development.

Noise survey data must include measurements taken during a rush-hour period and night time to determine the appropriate sound insulation measures necessary. The following noise criteria will be required to be achieved:

Bedrooms (night time - 23.00 - 07.00) 30 dB L Aeq (individual noise events shall not exceed 45 dB L Amax,F by more than 15 times)
 Living Rooms (daytime - 07.00 - 23.00) 35 dB L Aeq
 Gardens and terraces (daytime) 55 dB L Aeq

(b) Prior to the first occupation of the development, a verification report will be required to validate that the work undertaken conforms to the recommendations and requirements approved as part of part (a) of this planning condition. The verification report shall include post completion testing to confirm the noise criteria has been met. In instances of non-conformity, these shall be detailed along with mitigation measures required to ensure compliance with the noise criteria. A verification report and measures shall be agreed until such a time as the development complies with part (a) of this planning condition.

Any mitigation measures shall be implemented in accordance with a timescale to be agreed with the City Council, as Local Planning Authority, and thereafter retained and maintained in situ.

Reason: To secure a reduction in noise from traffic or other sources in order to protect future residents from noise disturbance pursuant to policies SP1, H1 and DM1 of the Core Strategy (2007) and saved policy DC26 of the Unitary Development Plan for the City of Manchester (1995).

21) The residential element of the development shall be carried out in accordance with the Waste Management Strategy by Curtins stamped as received by the City Council, as Local Planning Authority, on the 13 May 2021. The details of the approved scheme shall be implemented prior to the first use of the residential element and shall remain in situ whilst the use or development is in operation.

Reason - To ensure adequate refuse arrangement are put in place for the residential element of the scheme pursuant to policies EN19 and DM1 of the Manchester Core Strategy.

22) prior to the first use of each commercial/workspace unit as indicated on drawings L (--) Rev P8, L (--) 231 Rev P3 and L (--) 100 Rev P6 stamped as received by the City Council, as Local Planning Authority, on the 19 August and 13 May 2021 respectively, a scheme for the storage and management of waste shall be submitted for approval in writing by the City Council, as Local Planning Authority.

The details of the approved scheme shall be implemented prior to the first use of the each commercial/workspace unit and shall remain in situ whilst the use or development is in operation.

Reason - To ensure adequate refuse arrangement are put in place for the residential element of the scheme pursuant to policies EN19 and DM1 of the Manchester Core Strategy.

23) Prior to the first use of each of the workspace/commercial units, as indicated on drawings L (--) Rev P8, L (--) 231 Rev P3 and L (--) 100 Rev P6 stamped as received by the City Council, as Local Planning Authority, on the 19 August and 13 May 2021, should fume extraction be required, details of a scheme to extract fumes, vapours and odours from that commercial unit shall be submitted for approval in writing by the City Council, as Local Planning Authority. The approved scheme shall then be implemented prior to the first occupation of the commercial units and thereafter retained and maintained in situ.

Reason - To ensure appropriate fume extraction is provided for the commercial units pursuant to policies SP1 and DM1 of the Manchester Core Strategy and saved policy DC10 of the Unitary Development Plan for the City of Manchester (1995).

24) Prior to the first use of each of the Workspace/commercial units as indicated drawings L (--) Rev P8, L (--) 231 Rev P3 and L (--) 100 Rev P6 stamped as received by the City Council, as Local Planning Authority, on the 19 August and 13 May 2021, details of any roller shutters to the ground floor of that commercial unit shall be submitted for approval in writing by the City Council, as Local Planning Authority. The shutters shall be fitted internally to the premises. The approved details shall be implemented prior to the first occupation of each commercial units and thereafter retained and maintained in situ.

Reason - To ensure that the roller shutters are appropriate in visual amenity terms pursuant to policies SP1, EN1 and DM1 of the Manchester Core Strategy (2012).

25) The development hereby approved shall include a building and site lighting scheme and a scheme for the illumination of external areas during the period between dusk and dawn. Prior to the first occupation of the development, full details of such a scheme for the development shall be submitted for approval in writing by the City Council, as Local Planning Authority. The approved scheme shall be implemented in full prior to the first occupation of the development and shall remain in operation for so long as the development is occupied.

Reason - In the interests of amenity, crime reduction and the personal safety of those using and ensure that lighting is installed which is sensitive to the bat environment the proposed development in order to comply with the requirements of policies SP1 and DM1 of the Core Strategy.

26) If any lighting at the development hereby approved, when illuminated, causes glare or light spillage which in the opinion of the Council as local planning authority causes detriment to adjoining and nearby residential properties, within 14 days of a written request, a scheme for the elimination of such glare or light spillage shall be submitted to the Council as local planning authority and once approved shall thereafter be retained in accordance with details which have received prior written approval of the City Council as Local Planning Authority.

Reason - In order to minimise the impact of the illumination of the lights on the occupiers of nearby residential accommodation, pursuant to policies SP1 and DM1 of the Core Strategy.

27) Deliveries, servicing and collections including waste collections shall not take place outside the following hours:

Monday to Saturday 07:30 to 20:00

Sundays (and Bank Holidays): No deliveries/waste collections

Reason - In the interest of residential amenity pursuant to policies SP1 and DM1 of the Manchester Core Strategy (2012).

29) The workspace/commercial units hereby approved, as indicated on drawings L (--) Rev P8, L (--) 231 Rev P3 and L (--) 100 Rev P6 stamped as received by the City Council, as Local Planning Authority, on the 19 August and 13 May 2021, shall not be open outside the following hours:-

Monday to Saturday 08:00 to 23:30

Sundays (and Bank Holidays): 10:00 to 22:00

There shall be no amplified sound or any amplified music at any time within the unit.

Reason - In interests of residential amenity in order to reduce noise and general disturbance in accordance with saved policy DC26 of the Unitary Development Plan for the City of Manchester and policies SP1 and DM1 of the Core Strategy.

30) The workspace/commercial units as shown on drawings L (--) Rev P8, L (--) 231 Rev P3 and L (--) 100 Rev P6 stamped as received by the City Council, as Local Planning Authority, on the 19 August and 13 May 2021, shall remain as separate units and shall not be sub divided or amalgamated without the benefit of planning permission being secured.

Reason- In the interests of residential amenity and to ensure the future viability and vitality of the commercial units pursuant to saved policy DC26 of the Unitary Development Plan for the City of Manchester and policies DM1, C5 and SP1 of the Manchester Core Strategy.

31) The workspace/commercial units, as indicated on drawings L (--) Rev P8, L (--) 231 Rev P3 and L (--) 100 Rev P6 stamped as received by the City Council, as Local Planning Authority, on the 19 August and 13 May 2021, can be occupied as Use Class E (excluding convenience retail and a gymnasium) and Use Class F2 and for no other purpose of The Town and Country Planning (Use Classes) Order 1987 (or any order revoking and re-enacting that Order with or without modification). The first use of the commercial unit to be implemented shall thereafter be the permitted use of that unit

Reason - For the avoidance of doubt and in order to secure a satisfactory form of development due to the particular circumstance of the application site, ensuring the vitality of the units and in the interest of residential amenity, pursuant policy DM1 of the Core Strategy for Manchester .

32) Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) (England) Order 2015 (as amended) (or any order revoking and re-enacting that Order with or without modification) no part of the development shall be used for any purpose other than the purpose(s) of Class C3(a) of the Schedule to the Town and Country Planning (Use Classes) Order 1987 (as amended) (or in any provision equivalent to that Class in any statutory instrument revoking and re-enacting that Order with or without modification). For the avoidance of doubt, this does not preclude two unrelated people sharing a property.

Reason - In the interests of residential amenity, to safeguard the character of the area and to maintain the sustainability of the local community through provision of accommodation that is suitable for people living as families pursuant to policies DM1 and H11 of the Core Strategy for Manchester and the guidance contained within the National Planning Policy Framework.

33) The residential use hereby approved shall be used only as private dwellings (which description shall not include serviced properties or similar uses where sleeping accommodation (with or without other services) is provided by way of trade for money or money's worth and occupied by the same person for less than ninety consecutive nights) and for no other purpose (including any other purpose in Class C3 of the Schedule to the Town and Country Planning (Use Classes) Order 1987 (as amended), or any provision equivalent to that Class in any statutory instrument revoking and re-enacting that Order with or without modification).

Reason - To safeguard the amenities of the neighbourhood by ensuring that other uses which could cause a loss of amenity such as serviced apartments/apart hotels do not commence without prior approval; to safeguard the character of the area, and to maintain the sustainability of the local community through provision of accommodation that is suitable for people living as families pursuant to policies DM1 and H11 of the Core Strategy for Manchester and the guidance contained within the National Planning Policy Framework.

34) The development shall be carried out in accordance with the Crime Impact Statement and crime impact review stamped as received by the City Council, as Local Planning Authority, on the 13 May 2021 and 4 August 2021 respectively. The development shall only be carried out in accordance with these approved details. Prior to the first occupation of the development, the Council as Local Planning Authority must acknowledge in writing that it has received written confirmation of a Secured by Design accreditation.

Reason - To reduce the risk of crime pursuant to policies SP1 and DM1 of the Core Strategy and to reflect the guidance contained in the National Planning Policy Framework.

35) The development hereby approved shall be carried out in accordance with the Framework Travel Plan stamped as received by the City Council, as Local Planning Authority, on the 13 May 2020.

In this condition a Travel Plan means a document which includes:

- i) the measures proposed to be taken to reduce dependency on the private car by those living at the development;
- ii) a commitment to surveying the travel patterns of residents/staff during the first three months of the first use of the building and thereafter from time to time
- iii) mechanisms for the implementation of the measures to reduce dependency on the private car
- iv) measures for the delivery of specified Travel Plan services
- v) measures to monitor and review the effectiveness of the Travel Plan in achieving the objective of reducing dependency on the private car

Within six months of the first occupation of the development, a Travel Plan for the development which takes into account the information about travel patterns gathered pursuant to item (ii) above shall be submitted for approval in writing by the City Council as Local Planning Authority. Any Travel Plan which has been approved by the City Council as Local Planning Authority shall be implemented in full at all times when the development hereby approved is in use.

Reason - To assist promoting the use of sustainable forms of travel for residents, pursuant to policies T1, T2 and DM1 of the Manchester Core Strategy (2012).

36) Prior to the first occupation of the residential element, the cycle store shall be implemented in accordance drawings L (--) Rev P8 and L (--) 100 Rev P6 stamped as received by the City Council, as Local Planning Authority, on the 19 August and 13 May 2021 and thereafter retained and maintained in situ.

Reason - To ensure there is sufficient cycles stand provision at the development and the residents in order to support modal shift measures pursuant to policies SP1, T1, T2 and DM1 of the Manchester Core Strategy (2012).

37) Prior to the first occupation of the residential element of the development hereby approved, the car parking spaces, as indicated on drawings L (--) Rev P8 and L (--) 100 Rev P6 stamped as received by the City Council, as Local Planning Authority, on the 19 August and 13 May 2021 shall then be implemented, made available and remain in situ for as long as the development remains in use.

Reason - To ensure sufficient car parking is available for the occupants of the development pursuant to policies SP1, T1, and DM1 of the Manchester Core Strategy (2012).

38) Prior to the first occupation of the residential element of the development, a scheme of highway works and details of footpaths reinstatement/public realm for the development shall be submitted for approval in writing by the City Council, as Local Planning Authority.

This shall include the following:

- Signalised puffin crossing at Bradford Road (between Halmore Road and Layton Street);
- Amendment to parking restrictions on Bradford Road from single to double yellows;
- Provision of a car club bay in the vicinity of the site;
- Relocation of bus stop adjacent to southern car park entrance;
- Provision of tactile paving to vehicular access point;
- Resurfacing of footways around the application site; and
- Review and amendments to street lighting.

The approved scheme shall be implemented and be in place prior to the first occupation of the residential element of the development and thereafter retained and maintained in situ.

Reason - To ensure safe access to the development site in the interest of pedestrian and highway safety pursuant to policies SP1, EN1 and DM1 of the Manchester Core Strategy (2012).

39) Notwithstanding the TV Reception Survey by GTech, stamped as received by the City Council, as Local Planning Authority, on the 13 May 2021, within one month of the practical completion of the development, and at any other time during the construction of the development if requested in writing by the City Council as Local Planning Authority, in response to identified television signal reception problems within the potential impact area a study to identify such measures necessary to maintain at least the pre-existing level and quality of signal reception identified in the survey carried out above for the development shall be submitted for approval in writing by the City Council, as Local Planning Authority. The measures identified must be carried out either before the development is first occupied or within one month of the study being submitted for approval in writing to the City Council as Local Planning Authority, whichever is the earlier.

Reason - To provide an indication of the area of television signal reception likely to be affected by the development to provide a basis on which to assess the extent to which the development during construction and once built, will affect television reception and to ensure that the development at least maintains the existing level and quality of television signal reception - In the interest of residential amenity, as specified in policy DM1 of Core Strategy.

40) (a) Prior to the first occupation of the development, details, location and specification of 7 kw electric car charging points for 20% of the car parking space together with the remaining spaces fitted with infrastructure for future electric car charging capability shall be submitted for approval in writing by the City Council, as Local Planning Authority. The approved details shall then be implemented and be in place prior to the first occupation of the development and thereafter retained and maintained in situ.

(b) The number of fast charging electric car charging points shall be reviewed annually as part of the travel plan requirements of condition 35 of this planning permission (commencing from the date of this permission). The survey shall be completed within 7 days of each annual review date and the results of the survey provided to the City Council within 7 days thereafter. Any additional charging points identified as part of this review shall be implemented within two months of approval of the annual agreement.

Reason - In the interest of air quality pursuant to policies SP1 and EN16 of the Manchester Core Strategy (2012).

41) Prior to the first occupation of the development hereby approved, details of bird and bat boxes to be provided (including location and specification) for the development shall be submitted for approval in writing by the City Council, as Local Planning Authority. The approved details shall then be implemented within a timescale to be agreed in writing with the City Council, as Local Planning Authority.

Reason – To provide new habitats for birds and bats pursuant to policies SP1 and EN15 of the Manchester Core Strategy (2012).

42) Notwithstanding the General Permitted Development Order 2015 as amended by the Town and Country Planning (Permitted Development and Miscellaneous Amendments) (England) (Coronavirus) Regulations 2020 or any legislation amending or replacing the same, no further development in the form of upward extensions to the mid and corner buildings shall be undertaken other than that expressly authorised by the granting of planning permission.

Reason - In the interests of protecting residential amenity and visual amenity of the area in which the development is located pursuant to policies DM1 and SP1 of the Manchester Core Strategy.

43) Prior to the first occupation of the development, a signage strategy for the buildings shall be submitted for approval in writing by the City Council, as Local Planning Authority. The signage strategy will include timescales for implementation. The approved strategy shall then be implemented for the development and used to inform any future advertisement applications for the building.

Reason – In the interest of visual amenity pursuant to policies SP1 and DM1 of the Manchester Core Strategy (2012).

44) All windows at ground level with the exception of the WC and, unless shown otherwise on the approved drawings detailed in condition 2, shall be retained as a clear glazed window opening at all times and views into the premises shall not be screened or obscured in any way.

Reason - The clear glazed window(s) is an integral and important element in design of the ground level elevations and are important in maintaining a visually interesting street-scene consistent with the use of such areas by members of the public, and so as to be consistent with saved policy DC14 of the Unitary Development Plan for the City of Manchester and policies SP1 and DM1 of the Core Strategy.

45) The development hereby approved shall include for full disabled access to be provided to the internal courtyard and communal walkways and via the main entrances and to the floors above.

Reason - To ensure that satisfactory disabled access is provided by reference to the provisions Core Strategy policy DM1.

46) No doors (other than those designated as fire exits) shall open outwards onto adjacent pedestrian routes.

Reason – In the interest of pedestrian safety pursuant to policy DM1 of the Manchester Core Strategy (2012).

47) Prior to the first use of the development hereby approved, details of the siting, scale and appearance of the solar panels to the roof (including cross sections) as shall be submitted for approval in writing by the City Council, as Local Planning. The

approved details shall then be implemented prior to the first use of the development and thereafter retained and maintained in situ.

Reason - In the interest of ensuring the solar panels are installed and to ensure that they are appropriate in terms of visual amenity pursuant to policies SP1, EN1, EN6 and DM1 of the Manchester Core Strategy (2012).

48) All tree work should be carried out by a competent contractor in accordance with British Standard BS 3998 "Recommendations for Tree Work".

Reason - In order avoid damage to trees/shrubs adjacent to and within the site which are of important amenity value to the area and in order to protect the character of the area, in accordance with policies EN9 and EN15 of the Core Strategy.

49) Prior to above ground works, a scheme for the Biodiversity Mitigation and Enhancement Measures, as outlined in section 5 of The Extended Phase 1 Habitat Survey & Daytime Bat Survey by Rachel Hacking Ecology dated 2021, shall be submitted for approval in writing by the Local Planning Authority.

The approved scheme shall be implemented prior to first occupation of the development and shall be retained thereafter.

Reason – In the interest of improving biodiversity at the application site pursuant to policy EN15 of the Manchester Core Strategy (2012).

50) Prior to any above ground works, details of the ramp deck to provide level access to the commercial/workspaces within the internal courtyard shall be submitted for approval in writing by the City Council, as Local Planning Authority. This shall include sections and use of materials. The approved details shall be implemented and be completed prior to the first occupation of the residential element of the development.

Reason – In the interest of visual amenity pursuant to policies SP1, EN1, EN3 and DM1 of the Manchester Core Strategy (2012).

Informatives

It is expected that all modifications / improvements to the public highway are achieved with a maximum carbon footprint of 40%. Materials used during this process must also be a minimum of 40% recycled and fully recyclable. Developers will be expected to demonstrate that these standards can be met prior to planning conditions being discharged. The developer is to agree the above with MCC's Statutory Approvals and Network Resilience Teams post planning approval and prior to construction taking place.

Regarding S278 agreements a deposit is required to begin an application, additional costs will be payable and are to be agreed with S278 team. A S278 is required for works to the adopted highway, minimum standard S278 technical approval timescale is between 4-6 months, TRO's can take 10-12 months. An independent 'Stage 2'

Road Safety Audit will be required and the design may require changes if any issues are raised with all costs attributable to the Developer.

Listed Building Consent 130395/LO/2021

Recommendation Minded to Approve subject to the completion of the legal agreement associated with planning permission 130394/FO/2021

Article 35 Declaration

Officers have worked with the applicant in a positive and proactive manner based on seeking solutions to problems arising in relation to dealing with the planning application. Pre application advice has been sought in respect of this matter where early discussions took place regarding the siting/layout, scale, design and appearance of the development and impact on the listed building. Further work and discussion have taken place with the applicant through the course of the application. The proposal is considered to be acceptable and therefore determined within a timely manner.

Condition(s) to be attached to decision for approval

1) The development must be begun not later than the expiration of three years beginning with the date of this permission.

Reason - Required to be imposed pursuant to Section 18 of the Planning (Listed Buildings and Conservation Areas) Act 1990.

2) The demolition and alteration to Brunswick Mill shall not be undertaken before a contract for the carrying out of the building works have been made, and planning permission has been granted for the redevelopment for which the contract provides, and evidence of that contract has been supplied to the City Council as Local Planning Authority.

Reason - In the interests of visual amenity and for the avoidance of doubt, and to ensure that redevelopment of the site takes place following external alterations to the existing building pursuant to policy EN3 of the Manchester Core Strategy and saved DC19 of the Unitary Development Plan for the City of Manchester, and the National Planning Policy Framework.

3) The development hereby approved shall be carried out in accordance with the following drawings and documents:

Drawings

L (--) 002 Rev P6, L (--) 200 Rev P8, L (--) 2002 Rev P6, L (--) 231 Rev P3, L (--) 232 Rev P3, L (--) 233 Rev P3, L (--) 234 Rev P3, L (--) 436 Rev P4, L (--) 438 Rev P3, L (--) 510 Rev P4, L (--) 511 Rev P2, L (SK) 026, L (SK) 027 and 106-LYR-XX-XX-

M2D-L-0001-02 Rev 1 stamped as received by the City Council, as Local Planning Authority, on the 19 August 2021.

L (--) 202 Rev P7, L (--) 222 Rev P6 and 232 Rev P3 stamped as received by the City Council, as Local Planning Authority, on the 18 August 2021

L(--)100 Rev P6, L(--)101 Rev P5, L(--)102 Rev P5, L(--)103 Rev P5, L(--)104 Rev P5, L(--)105 Rev P5, L(--)106 Rev P5, L(--)107 Rev P5, L(--)108 Rev P5, L(--)203 Rev P5, L(--)204 Rev P5, L(--)205 Rev P5, L(--)206 Rev P5, L(--)207 Rev P5, L(--)220 Rev P5, L(--)221 Rev P5, L(--)222 Rev P5, L(--)223 Rev P4, L(--)224 Rev P4, L(--)225 Rev P4, L(--)226 Rev P4, L(--)227 Rev P4, L(--)400 Rev P3, L(--)410 Rev P3, L(--)415 Rev P1, L(--)420 Rev P3, L(--)430 Rev P2, L(--)431 Rev P2, L(--)432 Rev P2, L(--)433 Rev P2, L(--)435 Rev P2, L(--)437 Rev P3, L(--)439 Rev P2, L(--)470 Rev P2, L(--)471 Rev P1, L (--) 900 Rev P1, L(--)901 Rev P1, , L(--)902 Rev P1, L(--)910 Rev P2, L(--)915 Rev P1, L(--)916 Rev P1, L(--)920 Rev P2, L(--)921 Rev P1, , L(--)922 Rev P1 and L(--)B01 Rev P1 stamped as received by the City Council, as Local Planning Authority, on the 13 May 2021

Supporting Information

Design and Access Statement by Hodder and Partners, Landscape and Public Realm Strategy by Layer, Heritage Assessment by Stephen Levrant Heritage Architecture, Window Condition Report by Stephen Levrant Heritage Architecture, Archaeological Assessment by Salford University, Construction Methodology Statement by Civic, Structural Statement by Civic and Feasibility Report by Civic, stamped as received by the City Council, as Local Planning Authority, on the 13 May 2021

Reason - To ensure that the development is carried out in accordance with the approved plans. Pursuant to policies SP1 and DM1 of the Core Strategy.

4) No groundworks shall take place until the applicant or their agents or successors in title has secured the implementation of a programme of archaeological works. The works are to be undertaken in accordance with a Written Scheme of Investigation (WSI) submitted to and approved in writing by Manchester Planning Authority. The WSI shall cover the following:

1. Informed by the updated North West Regional Research Framework, a phased programme and methodology of investigation and recording to include:

- a historic building investigation (Level 2/3) of any historic buildings or structures that are to be removed
- archaeological evaluation trenching;
- pending the results of the above, targeted open-area excavation.

2. A programme for post-investigation assessment to include:

- production of a final report on the results of the investigations and their significance.

3. Deposition of the final report with the Greater Manchester Historic Environment Record.
4. Dissemination of the results of the archaeological investigations commensurate with their significance, including provision for a permanent information panel.
5. Provision for archive deposition of the report and records of the site investigation.
6. Nomination of a competent person or persons/organisation to undertake the works set out within the approved WSI.

Reason: To record and advance understanding of heritage assets impacted on by the development and to make information about the heritage interest publicly accessible pursuant to policy EN3 of the Manchester Core Strategy (2012) and saved policy DC20 of the Unitary Development Plan for the City of Manchester (1995).

5) Prior to the commencement of any works, features and fixtures previously not identified, but uncovered as part of the historic building survey required by condition 4, shall be considered and incorporated within the development where possible in accordance with a strategy submitted for approval in writing by the City Council, as Local Planning Authority. This shall include method statement(s) detailing the position and condition of the fixture/fittings and how to repair and retain them in situ or elsewhere with the mill complex. The approved strategy shall be implemented and a verification report submitted for approval prior to the first occupation of the development.

Reason – To identify all possible historic features within the building and retain them in situ pursuant to policy EN3 of the Manchester Core Strategy (2012) and saved policy DC20 of the Unitary Development Plan for the City of Manchester (1995).

6) Prior to the commencement of works to remove the following listed fabric, as indicated within the heritage statement prepared by Stephen Levrant stamped as received by the City Council, as Local Planning Authority, on the 13 May 2021, a detailed method statement shall be submitted detailing how the fabric will be removed together how the remaining historic fabric will be protected and repaired once removal takes place.

This condition applies to the following works:

- Removal of infills to blocked up windows and doors (to the mill, round headed openings to ground floor of west elevation, window openings to single storey extension (south east corner of mill, window openings to former engine house, window/door openings to courtyard, windows to 1920s extension of waste house);
- One bay to 1920 extension to third floor roof of north entrance block roof;
- 1950s extension to north east corner building;
- Modern extension to north west corner of mill;
- Later brick extension to west of elevation of former engine house;
- Later extension to courtyard;

- 1960s brick lift to east elevation;
- Removal 20th century internal walls/stud walls (ground and first floors)
- Single storey shed structure to south east corner.

The removal works shall be carried out in accordance with the approved details. Should any other parts of the Listed structure become damaged as a consequence of the removal/alteration work then such damage should be made good following a method of works previously agreed in writing by the City Council, as Local Planning Authority.

Reason - To ensure the method used to remove historic fabric is appropriate and that any damaged to the historic fabric is repaired to a satisfactory standard pursuant to policy EN3 of the Manchester Core Strategy (2012) and saved policy DC19 of the Unitary Development Plan for the City of Manchester (1995).

7) Prior to the commencement of repair works to the interior and exterior masonry and stonework including insertion of terracotta vents, a detailed method statement and specification (including material specification) for the removal, alteration, repair, re-pointing, cleaning and replacement works shall be submitted for approval in writing by the City Council, as Local Planning Authority.

The approved details and method statement shall be implemented as part of the development and completed prior to the first occupation of the development.

Best endeavours shall be made to use salvage brick work and stonework and other materials wherever possible in the works associated with the development. Should it not be possible to use salvage material in the restoration then notification shall be made to the City Council, as Local Planning Authority, prior to the starting of the works. Suitable materials shall then be agreed with the City Council, as Local Authority, and used where necessary.

Reason - To ensure that a satisfactory restoration and repair of the exterior of the listed building in order to ensure that the historic fabric is retained where possible and appropriate materials used in the restoration of the Listed Building pursuant to policy EN3 of the Manchester Core Strategy (2012) and extant policy DC19 of the Unitary Development Plan for the City of Manchester (1995) and the NPPF/NPPG.

8) Prior to the commencement of repair works to the original internal staircases, a detailed method statement and specification (including material specification) for the removal, alteration, repair, cleaning and replacement works shall be submitted for approval in writing by the City Council, as Local Planning Authority.

The approved details and method statement shall be implemented as part of the development and completed prior to the first occupation of the development.

Best endeavours shall be made to use salvage and re-use existing features, stone/brickwork and other materials wherever possible in the works associated with the development. Should it not be possible to use salvage material in the restoration then notification shall be made to the City Council, as Local Planning Authority, prior

to the starting of the works. Suitable materials shall then be agreed with the City Council, as Local Authority, and used where necessary.

Reason - To ensure that a satisfactory restoration and repair of the listed building in order to ensure that the historic fabric is retained where possible and appropriate materials used in the restoration of the Listed Building pursuant to policy EN3 of the Manchester Core Strategy (2012) and extant policy DC19 of the Unitary Development Plan for the City of Manchester (1995) and the NPPF/NPPG.

9) Prior to the commencement of repair works to the Jack arches, a detailed method statement and specification (including material specification) for the removal, alteration, repair, cleaning, replacement and fire protection works shall be submitted for approval in writing by the City Council, as Local Planning Authority.

The approved details and method statement shall be implemented as part of the development and completed prior to the first occupation of the development.

Best endeavours shall be made to use salvage and re-use existing features, brickwork and other materials wherever possible in the works associated with the development. Should it not be possible to use salvage material in the restoration then notification shall be made to the City Council, as Local Planning Authority, prior to the starting of the works. Suitable materials shall then be agreed with the City Council, as Local Authority, and used where necessary.

Reason - To ensure that a satisfactory restoration and repair of the listed building in order to ensure that the historic fabric is retained where possible and appropriate materials used in the restoration of the Listed Building pursuant to policy EN3 of the Manchester Core Strategy (2012) and extant policy DC19 of the Unitary Development Plan for the City of Manchester (1995) and the NPPF/NPPG.

10) Prior to the commencement of works to the internal columns (including ceiling details), a detailed method statement and specification (including material specification) for the exposure of the columns, alteration, repair, cleaning, and fire protection works shall be submitted for approval in writing by the City Council, as Local Planning Authority.

The approved details and method statement shall be implemented as part of the development and completed prior to the first occupation of the development.

Reason - To ensure that a satisfactory restoration and repair of the listed building in order to ensure that the historic fabric is retained where possible and appropriate materials used in the restoration of the Listed Building pursuant to policy EN3 of the Manchester Core Strategy (2012) and extant policy DC19 of the Unitary Development Plan for the City of Manchester (1995) and the NPPF/NPPG.

11) Prior to the commencement of works to the structural ties, a detailed method statement and specification (including material specification) for the exposure of the ties, alteration, repair, cleaning, and fire protection works shall be submitted for approval in writing by the City Council, as Local Planning Authority.

The approved details and method statement shall be implemented as part of the development and completed prior to the first occupation of the development.

Reason - To ensure that a satisfactory restoration and repair of the listed building in order to ensure that the historic fabric is retained where possible and appropriate materials used in the restoration of the Listed Building pursuant to policy EN3 of the Manchester Core Strategy (2012) and extant policy DC19 of the Unitary Development Plan for the City of Manchester (1995) and the NPPF/NPPG.

12) Prior to the commencement of works to install any relevant mechanical and electrical (M & E) installations (including CCTV and lighting) attached to any listed fabric, detailed plans, method statement and specification showing the location, profile, fixing, sections and suitable samples, where necessary shall be submitted for approval in writing by the City Council, as Local Planning Authority. This shall include details of the removal, alterations and repair of the historic fabric of the building.

The development shall then be carried out in accordance with the details approved and thereafter be retained as approved throughout the life of the development. Should the M & E no longer be required, they should be removed and the elevations should be made good following a scheme previously approved in writing by the City Council, as Local Planning Authority.

Reason:- To ensure the M & E at the development does not harm the Listed structure and is attached appropriately to the historic fabric pursuant to policy EN3 of the Manchester Core Strategy (2012) and extant policy DC19 of the Unitary Development Plan for the City of Manchester (1995).

13) Prior to the installation new and doors windows within the existing listed façade, a detailed method statement and specification of the windows (including frame size, fenestration and design together with how they will be fitted and fixed to the historic fabric - appropriate sections, elevations and materials) shall be submitted for approval in writing by the City Council, as Local Planning Authority. This should also include any removal, alteration and repair of the historic fabric.

The works shall then be carried out in accordance with this method statement and specification as part of the development and be completed prior to the first occupation of the commercial units.

Reason - To ensure that a satisfactory interventions into the Listed Building and retention/repair of as much of the Listed fabric as possible along with appropriate use of materials in the restoration of the Listed Buildings pursuant to policy EN3 of the Manchester Core Strategy (2012) and extant policy DC19 of the Unitary Development Plan for the City of Manchester (1995) and the NPPF/NPPG.

14) Prior to the commencement of works to the roof to the north entrance block, 1920s skylights to roof of north entrance block, roof of original mill and roof of former engine house a detailed method statement and specification for the works shall be submitted for approval in writing by the City Council, as Local Planning Authority

(including methods for undertaking works, use of materials, fixing details and sections where appropriate).

The approved details and method statement shall be implemented as part of the development and completed prior to the first occupation of the development.

Reason - To ensure that a satisfactory interventions into the Listed Building and to ensure that the repair and maintenance work is suitable and appropriate for the restoration of the ventilation stack pursuant to policy EN3 of the Manchester Core Strategy (2012) and extant policy DC19 of the Unitary Development Plan for the City of Manchester (1995) and the NPPF/NPPG.

15) Prior to the commencement of works reinstate rainwater goods to the listed building, a detailed method statement, specification and location of rainwater goods including how they will be fixed to the listed fabric, shall be submitted for approval in writing by the City Council, as Local Planning Authority. The installation of the rainwater goods shall be carried out in accordance with this method statement and be completed prior to the first occupation of the development.

Reason - To ensure the rainwater goods are of an acceptable appearance and there is no resulting harm to the Listed building pursuant to policy EN3 of the Manchester Core Strategy (2012).

16) Prior to the installation of the gates to Bradford Road, a detailed method statement, specification and location of the gates including how they will be fixed to the listed fabric, shall be submitted for approval in writing by the City Council, as Local Planning Authority. The installation of the gates shall be carried out in accordance with this method statement and be completed prior to the first occupation of the development.

Reason - To ensure the gates to Bradford Road are of an acceptable appearance and there is no resulting harm to the Listed building pursuant to policy EN3 of the Manchester Core Strategy (2012).

17) Prior to the commencement of works to repair the servicing original areas of painted stucco, a detailed method statement and specification for the works shall be submitted for approval in writing by the City Council, as Local Planning Authority (including methods for undertaking works and use of materials).

The approved details and method statement shall be implemented as part of the development and completed prior to the first occupation of the development.

Reason - To ensure that a satisfactory interventions into the Listed Building and to ensure that the repair and maintenance work is suitable and appropriate for the restoration of the ventilation stack pursuant to policy EN3 of the Manchester Core Strategy (2012) and extant policy DC19 of the Unitary Development Plan for the City of Manchester (1995) and the NPPF/NPPG.

18) Prior to the commencement of works to remove the fire escapes from the internal courtyard elevations, a detailed method statement for the removal shall be submitted

for approval in writing by the City Council, as Local Planning Authority including details to retain and repair the brackets of the fire escapes in situ.

The works to the fire escapes shall be carried out in accordance with this method statement and specification and be completed prior to the first occupation of the development.

Reason - To ensure the removal of the fire escapes is acceptable, ensure that historic fabric can be retained where possible and there is no resulting harm to the Listed building pursuant to policy EN3 of the Manchester Core Strategy (2012).

19) Prior to the creation of the new concierge entrance to the north courtyard elevation and centre of the southern block, a detailed method statement, specification and location entrance including how the opening would be created and the new entrance fixed to the listed fabric, shall be submitted for approval in writing by the City Council, as Local Planning Authority. The installation of the new entrance shall be carried out in accordance with this method statement and be completed prior to the first occupation of the development.

Reason - To ensure the entrances are of an acceptable appearance and there is no resulting harm to the Listed building pursuant to policy EN3 of the Manchester Core Strategy (2012).

20) Prior to the removal of the courtyard cobbles, a detailed method statement for the removal, temporary storage and re-laying of the cobbles within the courtyard shall be submitted for approval in writing by the City Council, as Local Planning Authority. The removal, temporary storage and re-laying of the cobbles shall be carried out in accordance with this method statement and be completed prior to the first occupation of the development.

Reason – In the interest of preserving the cobbles whilst they are temporarily removed from site and that they are re-instated to an acceptable standard pursuant to policy EN3 of the Manchester Core Strategy (2012).

21) Prior to the commencement of works associated with the new vertical risers, a detailed method statement and specification for the works shall be submitted for approval in writing by the City Council, as Local Planning Authority. This shall include details of the how openings shall be created within the listed building in order to minimise unnecessary removal of listed fabric. The approved details shall be implemented as part of the development and be completed prior to the first occupation of the development. Should any parts of the Listed Building become damaged as a consequence of the works then such damage should be made good following a method of works previously agreed in writing by the City Council, as Local Planning Authority.

Reason - To ensure the installation of the risers is appropriate and does not cause and harm or damage to the fabric of the Listed Building and that any damaged to the building which result as a consequence is repaired to a satisfactory standard pursuant to policy EN3 of the Manchester Core Strategy (2012) and saved policy DC19 of the Unitary Development Plan for the City of Manchester (1995).

22) Prior to the commencement of works associated with the new staircases to north entrance block, a detailed method statement and specification for the works shall be submitted for approval in writing by the City Council, as Local Planning Authority. This shall include details of the how openings shall be created within the listed building in order to minimise unnecessary removal of listed fabric. The approved details shall be implemented as part of the development and be completed prior to the first occupation of the development. Should any parts of the Listed Building become damaged as a consequence of the works then such damage should be made good following a method of works previously agreed in writing by the City Council, as Local Planning Authority.

Reason - To ensure the installation of the staircases is appropriate and does not cause and harm or damage to the fabric of the Listed Building and that any damaged to the building which result as a consequence is repaired to a satisfactory standard pursuant to policy EN3 of the Manchester Core Strategy (2012) and saved policy DC19 of the Unitary Development Plan for the City of Manchester (1995).

23) Prior to the commencement of works associated with the new lifts, a detailed method statement and specification for the works shall be submitted for approval in writing by the City Council, as Local Planning Authority. This shall include details of the how openings shall be created within the listed building in order to minimise unnecessary removal of listed fabric. The approved details shall be implemented as part of the development and be completed prior to the first occupation of the development. Should any parts of the Listed Building become damaged as a consequence of the works then such damage should be made good following a method of works previously agreed in writing by the City Council, as Local Planning Authority.

Reason - To ensure the installation of the lifts is appropriate and does not cause and harm or damage to the fabric of the Listed Building and that any damaged to the building which result as a consequence is repaired to a satisfactory standard pursuant to policy EN3 of the Manchester Core Strategy (2012) and saved policy DC19 of the Unitary Development Plan for the City of Manchester (1995).

24) Prior to the commencement of works to install new raised floors within the listed building, details shall be submitted outlining areas where the existing quarry tiles can be exposed throughout the listed building together with a detailed method statement, specification and repair strategy for exposed areas of tiles shall be submitted for approval in writing by the City Council, as Local Planning Authority.

Once the areas of exposed tiles have been established, this should inform the specification and details of the raised floor and details including sections shall be submitted for approval in writing by the City Council, as Local Planning Authority prior to the commencement of any works to the floors.

The works to the floors including associated repair work and areas of new flooring shall be carried out in accordance with this method statement and be completed prior to the first occupation of the development.

Reason - To ensure that the original floor is exposed where possible and areas of new flooring are appropriate and minimise impact on the listed building pursuant to policy EN3 of the Manchester Core Strategy (2012) and saved policy DC19 of the Unitary Development Plan for the City of Manchester (1995).

25) Prior to the commencement of works associated with the installation of the new internal partitions, circulation cores and internal wall insulation of the development (commercial and residential elements), a detailed method statement and specification (including sections, elevations and materials) in association with the installation works (including the removal, alteration and repair of the historic fabric), shall be submitted for approval in writing by the City Council, as Local Planning Authority.

The works shall then be carried out in accordance with this method statement and specification as part of the development and be completed prior to the first occupation of the development.

Reason - To ensure that satisfactory interventions into the Listed Building and retention/repair of as much of the Listed fabric as possible along with appropriate use of materials in the restoration of the Listed Buildings pursuant to policy EN3 of the Manchester Core Strategy (2012) and extant policy DC19 of the Unitary Development Plan for the City of Manchester (1995) and the NPPF/NPPG.

26) Prior to the commencement of the works to the former canteen tiles to the north east corner of the third floor, a detailed method statement and specification for the repair, restoration, removal and extent of areas to be exposed within the building shall be submitted for approval in writing by the City Council, as Local Planning Authority. The works must be carried out in accordance with this method statement and specification and be completed prior to the first occupation of the development.

Reason - To ensure that the works to the tiles is appropriate and is done so in a manner which does not harm the fabric of the Listed Building and to ensure that the resulting condition of the buildings which will be revealed is repaired appropriately within a suitable time frame pursuant to policy EN3 of the Manchester Core Strategy (2012) and extant policy DC19 of the Unitary Development Plan for the City of Manchester (1995) and the NPPF/NPPG.

27) Prior to the commencement of works to the retained transformer, waste and engine buildings, a detailed specification and method statement (including material specification) for the removal, alteration, repair, re-pointing, cleaning and replacement works shall be submitted for approval in writing by the City Council, as Local Planning Authority.

The approved details and method statement shall be implemented as part of the development and completed prior to the first occupation of the development.

Best endeavours shall be made to use salvage brick work and stonework and other materials wherever possible in the works associated with the development. Should it not be possible to use salvage material in the restoration then notification shall be made to the City Council, as Local Planning Authority, prior to the starting of the

works. Suitable materials shall then be agreed with the City Council, as Local Authority, and used where necessary.

Reason - To ensure that a satisfactory restoration and repair of the exterior of the listed building in order to ensure that the historic fabric is retained where possible and appropriate materials used in the restoration of the Listed Building pursuant to policy EN3 of the Manchester Core Strategy (2012) and extant policy DC19 of the Unitary Development Plan for the City of Manchester (1995) and the NPPF/NPPG.

28) Prior to the commencement of works to install the ramp deck to provide level access to the commercial/workspaces within the internal courtyard, a detailed method statement including sections, use of material and how the ramp would be attached to listed fabric shall be submitted for approval in writing by the City Council, as Local Planning Authority. The approved details shall be implemented and be completed prior to the first occupation of the residential element of the development.

Reason – In the interest of ensuring the impact on the setting and fabric of the listed building is minimised pursuant to policies SP1, EN1, EN3 and DM1 of the Manchester Core Strategy (2012).

29) Prior to the commencement of works to install the steps to the Ashton Canal a detailed method statement including sections, use of material and how the steps would be attached to listed fabric shall be submitted for approval in writing by the City Council, as Local Planning Authority. The approved details shall be implemented and be completed prior to the first occupation of the residential element of the development.

Reason – In the interest of ensuring the impact on the setting and fabric of the listed building is minimised pursuant to policies SP1, EN1, EN3 and DM1 of the Manchester Core Strategy (2012).

Local Government (Access to Information) Act 1985

The documents referred to in the course of this report are either contained in the file(s) relating to application ref: 130394/FO/2021 held by planning or are City Council planning policies, the Unitary Development Plan for the City of Manchester, national planning guidance documents, or relevant decisions on other applications or appeals, copies of which are held by the Planning Division.

The following residents, businesses and other third parties in the area were consulted/notified on the application:

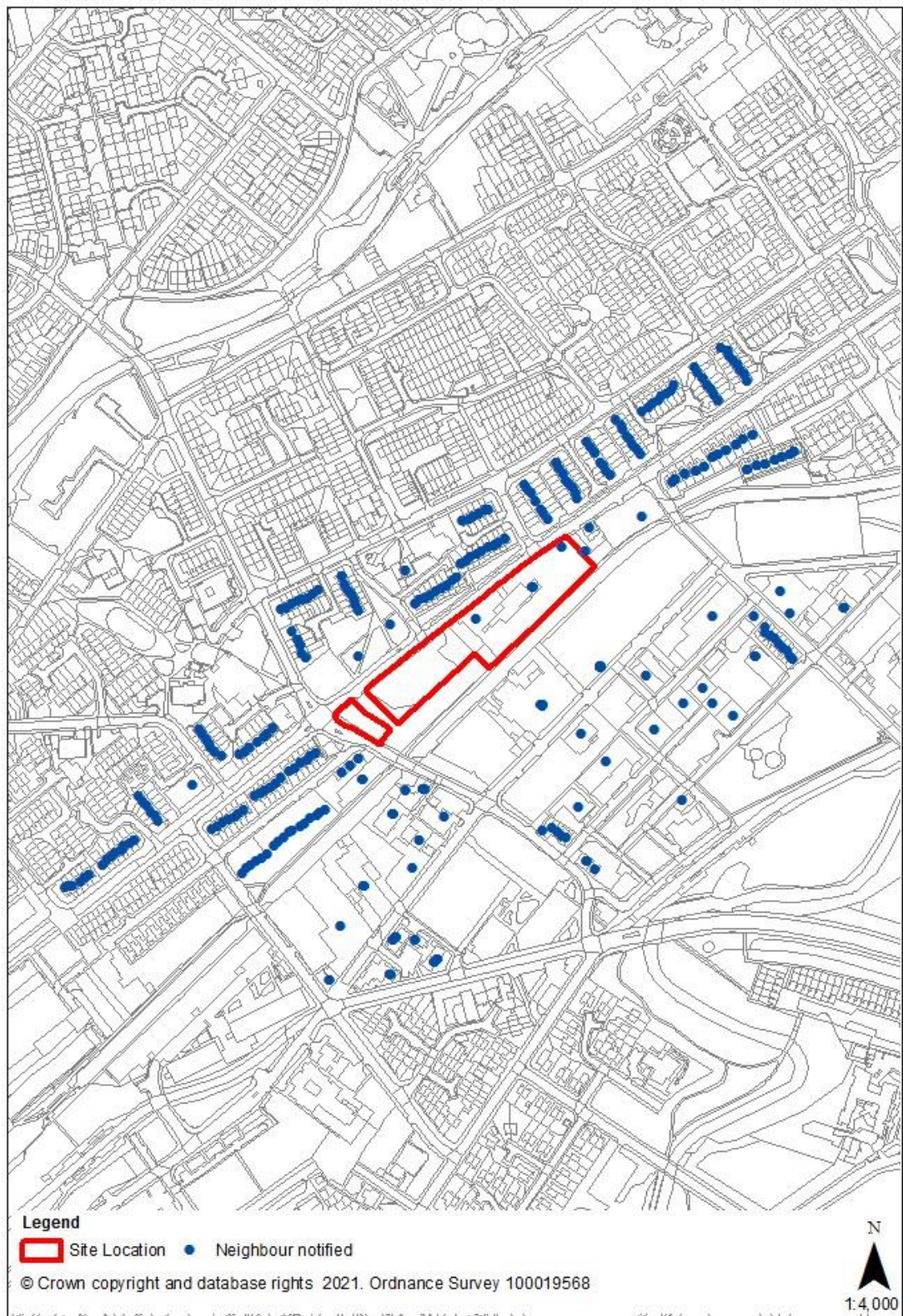
**Highway Services
Environmental Health
Neighbourhood Team Leader (Arboriculture)
MCC Flood Risk Management
Strategic Development Team
Work & Skills Team
Greater Manchester Police**

Historic England (North West)
Environment Agency
Transport For Greater Manchester
Greater Manchester Archaeological Advisory Service
United Utilities Water PLC
Canal & River Trust
National Amenity Societies
Greater Manchester Ecology Unit

A map showing the neighbours notified of the application is attached at the end of the report.

Representations were received from the following third parties:

Relevant Contact Officer :	Jennifer Atkinson
Telephone number :	0161 234 4517
Email :	jennifer.atkinson@manchester.gov.uk



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